



# **The City Of East Peoria Public Right-Of-Way Accessibility Transition Plan**

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By



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## **Introduction**

The Illinois Department of Transportation's (IDOT) Bureau of Local Roads (BLR) Manual states the following in Chapter 8 when describing the Public Right-Of-Way Accessibility Transition Plan. All portion of the IDOT BLR Manual are italicized for clarity.

*The Americans with Disabilities Act (ADA) is a civil rights statute that protects qualified individuals with disabilities from discrimination on the basis of disability. Title II of the ADA prohibits discrimination in all services, programs, and activities provided to persons with disabilities by State and local governments, including public transportation. The ADA applies to all facilities built before and after 1990. Local public agencies (LPA) are required to perform self-evaluations of their current facilities, relative to the accessibility requirements of the ADA. LPAs are required to correct any deficiencies identified through the self-evaluation. Only LPAs with more than 50 employees (including both full and part-time employees) are required to have a public right-of-way (PROW) accessibility transition plan detailing how and the deficiencies will be corrected is recommended. However, a public right-of-way (PROW) accessibility transition plan is recommended for all LPAs.*

### **1.0        Self-Evaluation**

#### **1.0(a)        General**

The IDOT BLR Manual states...*A site must contain at least one pedestrian access route within the boundary of the site from public transportation stops, accessible parking, accessible passenger loading zones, and public streets or sidewalks, to the pedestrian access route for the building they serve. However, LPAs are not required to install pedestrian facilities on the PROW as a requirement under the ADA. Once a pedestrian facility is constructed, the LPA shall provide an accessible pedestrian access route within the pedestrian facility. On sidewalks, a pedestrian access route should not alternate between one side of a highway and the other unless caused by temporary pedestrian re-routing due to construction of sidewalks and/or curbs and gutters.*

#### **1.0(b)        Inventory**

The IDOT BLR Manual states...*LPAs shall conduct an inventory of existing pedestrian facilities owned, maintained, or operated by the LPA. Inventory approaches are on-ground surveys, windshield surveys, aerial photo studies, or drawing reviews. Inventories are eligible for Federal, State, and MFT funding (See Section 4-3.06).*

The City of East Peoria, the Tri-County Regional Planning Commission, and Patrick N. Meyer & Associates, Inc. conducted an extensive survey of the existing sidewalks throughout the City of East Peoria. The data collected included condition, photos, and dimensions. The data is available for viewing on the City of East Peoria's website.

### 1.0(c) Barrier Identification

The IDOT BLR Manual states...*Barriers on pedestrian access routes should be identified in the self-evaluation. This also may require coordinating with local disability organizations and the public. When prioritizing the corrective actions, consider the following:*

- *location of pedestrian generators, particularly those in high-priority areas that would be likely serve individuals with disabilities (e.g., medical facilities, high-rise buildings, housing for the elderly, nursing homes, libraries, and commercial or government buildings),*
- *location of existing pedestrian access routes,*
- *location of existing utilities, signs, poles, or other features (e.g., steps) that would need to be removed to provide full accessibility,*
- *existing ground contours that would affect the longitudinal and transverse slope of sidewalks and ramps,*
- *location of marked crosswalks, and*
- *presence of drainage features (e.g., inlets, manholes).*

*The figure below provides some possible barriers for facilities on the public rights-of-way.*

<i>Issue</i>	<i>Possible Barriers</i>
<i>Sidewalk and Pathway Clear Width</i>	<i>Narrow, Obstructions (see below), Protruding Objects</i>
<i>Sidewalk and Pathway Cross Slope</i>	<i>Steepness, Irregularity, Variability, Warping, Settlement</i>
<i>Landings Along Sidewalks and Pathways</i>	<i>Less Than 4 feet by 4 feet</i>
<i>Sidewalk and Pathway Grade</i>	<i>Steepness, Angle Points</i>
<i>Materials and Finishes</i>	<i>Deterioration of Surfaces, Deterioration of Markings, Appropriateness of Material (ex. Cobblestones)</i>
<i>Gratings</i>	<i>Grating Type, Grate Opening Orientation</i>
<i>Discontinuities</i>	<i>Missing Sections, Gaps, Drops, Steps</i>
<i>Detectable Warning System</i>	<i>Missing, Inappropriate Materials, Inadequate Size, Wrong Location</i>
<i>Obstructions</i>	<i>Signs, Mail Boxes, Fire Hydrants, Benches, Telephones, Traffic Signal Poles, Traffic Signal Controller Boxes, Newspaper Boxes, Drainage Structures, Trees, Grates, Pole Mounted Objects, Standing Water, Snow or Ice</i>
<i>Traffic Signal Systems</i>	<i>Lack of Provision for the Visually Impaired such as APS, Inadequate Time Allowed, Inoperable Buttons, Inaccessible Buttons</i>

<i>Curb Ramp</i>	<i>Missing, Doesn't Fall within Marked Crosswalk, Doesn't Conform to Guidelines</i>
<i>Curb Ramp Flares</i>	<i>Missing Where Required, Too Steep</i>

## ***PUBLIC RIGHTS-OF-WAY FACILITIES POTENTIAL ACCESSIBILITY BARRIERS***

### **Self-Evaluation**

Not unlike most local agencies, the City of East Peoria is facing an aging infrastructure with ever-decreasing budgets. The inventory of the sidewalks allowed a snapshot of the condition of the sidewalk and brought some significant issues to the forefront. Please see the issues below and the recommendations for such issues.

One notable item is what to do with deteriorated sidewalks that are not traversable; i.e. not compliant because of vertical lifts (tripping hazards), missing sidewalk/ramps, or other issues. This type of deteriorated/missing sidewalk/ramp is a barrier for the pedestrians and may be a liability for the City. We would recommend that the City have further discussions with legal counsel regarding the condition of existing sidewalk/ramps and the proper course of action to minimize liability while maximizing public safety and convenience. A site visit with legal counsel on some key locations in the City area is advisable. See attached **Exhibit A** for examples of sidewalk and ramp conditions in the City.

Also see **Exhibit A** for a table identifying the ramp conditions. In regards to ramps, there are approximately 700 ramps, of which 80% are considered safe but may not be fully compliant...20% of the locations either have no ramps or unsafe ramps. It is estimated to cost approximately \$5,000 per location in order to install safe compliant ramps; there are 142 non-existent or unsafe ramps which would calculate approximately \$710,000. These ramps may be further prioritized by the column of the spreadsheet that specifies.

A-Disabled population presence

B-High volume of pedestrians

C-Near public buildings or businesses

D-Low volume of pedestrians

Similarly, there is a table in **Exhibit A** identifying sidewalk condition ratings. In all, there are approximately 55% of the sidewalks within the City that are in good shape while the other 45% have varying degrees of deterioration. Another section of **Exhibit A** has a listing of vertical lifts (i.e. potential "tripping hazards") and obstructions. The rating system utilized was adapted from another local agency and is represented in **Exhibit A**.

Also like most local agencies, the City of East Peoria also has existing sidewalks that may be traversable but not technically up to the ADA standards. For example, ADA requires that any new sidewalks be built at a 2.0% cross-slope. There are sidewalks within the City that do not meet the 2.0% cross-slope. There are specific slopes and surfaces for ADA ramps. There are ramps within the City that do not meet those specific ADA ramp qualities. A concentrated effort on the inspection of sidewalks should enhance the compliance of all new or rehabilitated sidewalk. See attached **Exhibit B** for a reference to an IDOT presentation that references key issues in ADA compliance..

A survey of various questions regarding sidewalk issues was conducted online. See attached **Exhibit C** for the full copy of the survey and results. Some of the highlights of the survey are as follows.

- 1) When asked to arrange priorities, Police, fire, and roadway ranked higher than sidewalks.
- 2) Over 70% of the people who responded would like to see more funding put towards sidewalks.
- 3) The respondents ranked roadways as a likely source of sidewalk funding when compared to taking police and fire funding. A tax increase was the least acceptable method for funding sidewalks.
- 4) 6 out of 10 of the respondents wanted to see existing deteriorated sidewalks replaced either by utilizing existing revenue sources or creating a new one. 4 out of 10 respondents preferred to seem removed and returned to grass.
- 5) Over 70% of the survey replies indicated that they nearly three-quarters of respondents were in favor of the “wheel and spokes” sidewalk vision. The proposed strategy could limit pedestrian accessibility within neighborhoods, but would increase mobility for those wishing to walk or bicycle toward the City’s core. The final version of the map is included in this report in **Exhibit D**.

Walking areas adjacent to schools have been established as a GIS layer. These areas were created as an area where students could walk to school on proposed sidewalks adjacent to local streets without crossing any major or heavy traffic areas. These school walking areas were intended to promote an area for sidewalks to be considered for students to increase physical activity and promote a healthy/active lifestyle. There are Safe Routes to School grants thru IDOT for kindergarten thru 8<sup>th</sup> grade that could help this concept become a reality. It was noted from our discussions with the school that high school students within a certain distance are not bused and are encouraged to walk. Special attention should be paid to walking areas surround the high school. Further discussions with the individual schools and the School Districts are encouraged.

On a related note, a GIS layer for bus stops was also created. For those students who must ride the bus, it is the intention of the City to make a safe route for those students to travel to and from the bus stop. Again, further discussions with the individual schools and the School District are encouraged. Please see the attached **Exhibit E** for the school and bus stop related information.

#### Recommendations:

- 1) Existing deteriorated sidewalks throughout the City.
  - a) Have further discussions with legal counsel regarding liability and contemplate obligation to the citizens to perform one of the following (in order of priority).
    - i) Identify a specific source of funding to remove and replace sidewalk. Preference to be given to sidewalks needing repair/replacement in underserved neighborhoods.
    - ii) Let deteriorated sidewalks remain as is.
    - iii) Remove deteriorated sidewalks and return to grass
- 2) New or proposed sidewalks.
  - a) Identify the persons that will be inspecting the sidewalks and driveways. Ensure they have a complete understanding of current ADA regulations through education and seminars. Demand ADA compliance to the maximum extent possible.
- 3) Future Pedestrian Movement.
  - a) Propose the “wheel and spokes” concept as the future plan for pedestrian movement throughout the City.
  - b) Diligently engage in discussions with IDOT, state, and federal elected officials regarding the desire for the future concept.
- 4) Student travel to and from school for kindergarten through 8<sup>th</sup> grade.
  - a) Organize additional meetings with the schools. Secure a commitment from the school as to the desire for sidewalks within the adjacent walking areas noted on the map.
  - b) If a desire is there, actively apply for funding to promote the commitment to sidewalks.

- 5) Student travel to and from school for high school.
  - a) Organize additional meetings with the schools. Secure a commitment from the school as to the desire for sidewalks within the adjacent walking areas noted on the map.
  - b) If a desire is there, actively apply for funding to promote the commitment to sidewalks. Knowing that high school level sidewalk funding is scarce...identify a specific source of funding that will improve safe travel for the high school students.

## **2.0                    Public Right-of-way Accessibility Transition Plan Components**

### **2.0(a)                Designate ADA Coordinator**

*The IDOT BLR Manual states...LPAs must designate at least one responsible employee to coordinate ADA compliance for public rights-of-way (PROW). This may be the ADA Coordinator required by 55 ILCS 5/5-1131, 60 ILCS 1/85-60, or 65 ILCS 5/1-1-12, or may be an employee within the highway department. This position must be familiar with the LPAs operation, be properly trained in ADA PROW requirements, and be able to effectively communicate with governmental agencies, advocacy groups, and the public.*

The City of East Peoria's ADA Coordinator is the Supervisor of Streets. The contact information is stated below.

Ric Semonski  
Supervisor of Streets  
Department of Public Works  
2232 East Washington Street  
East Peoria, IL 61611  
Office: 309-698-4716  
Email: [RickSemonski@cityofeastpeoria.com](mailto:RickSemonski@cityofeastpeoria.com)

### **2.0(b)                Public Notice**

*LPAs must provide the public an opportunity to provide input on the PROW accessibility transition plan prior to adoption. The target audience should include public transit users and disability advocacy groups as well as the general public. LPAs should determine the most effective way to provide notice. If notice is provided via a website, the website must in itself be accessible.*

The City of East Peoria provided an opportunity for the public to provide input on the this plan at the City's website link below.

A copy of the website page is presented in Exhibit F.

### **2.0(c)                Grievance Procedure**

*LPAs must adopt and publish procedures for resolving grievances arising under Title II of the ADA. The procedures are intended to set out a system for resolving complaints of disability discrimination in a prompt*

*and fair manner. The grievance procedure provides an opportunity to resolve a local issue at the local level. However, the use of LPAs grievance procedure is not a prerequisite to filing a complaint with either a federal agency or a court.*

The City of East Peoria's grievance procedure is provided via the website address below. See the attached screenshot of the website in Exhibit G.

#### **2.0(d) Design Standards, Specifications, and Details**

*LPAs must adopt technical guidelines that comply with ADA Standards. LPAs may adopt nationally accepted technical guidelines (i.e. PROWAG) or develop their own. LPAs may use Section 41-6 of the BLRS Manual to meet this requirement.*

The City of East Peoria should consider utilizing Section 41-6 of the BLRS Manual to meet this requirement...this section is in Exhibit H.

#### **2.0(e) Self-Evaluation**

*LPAs shall include the self-evaluation as part of the PROW accessibility transition plan.*

The City of East Peoria has completed a sidewalk inventory throughout the City. The following website contains the GIS link of the inventory of the sidewalk system.

#### **2.0(f) Schedule and Budget for Improvements**

*LPAs must include a schedule of improvements to upgrade accessibility annually in accordance with the PROW accessibility transition plan. Upgrades may be performed as individual projects or as part of regularly scheduled maintenance or construction projects. Any alteration of existing facilities or construction of new facilities must comply with ADA. Federal, State, and Motor Fuels Tax funds may be used to comply with ADA.*

*Generally, priority should be given to transportation facilities, public places, and places of employment. Other factors to consider when prioritizing improvements may include:*

- *Severity of non-compliance,*
- *Citizen requests or complaints regarding inaccessible locations,*
- *Pedestrian level of service,*
- *Population density,*
- *Presence of a disabled population, and/or*
- *Cost*

The City evaluates the necessity and the budget for sidewalk improvements based on all of the factors above. The City performs sidewalk improvements by its own public works staff, hiring outside contractors, and enforcing ADA compliance by commercial/residential developers. The schedule and

budget for both inside and outside improvements depend on the available funds including state and federal grants/loans. The local development economy is largely the catalyst for compliance by commercial/residential developers.

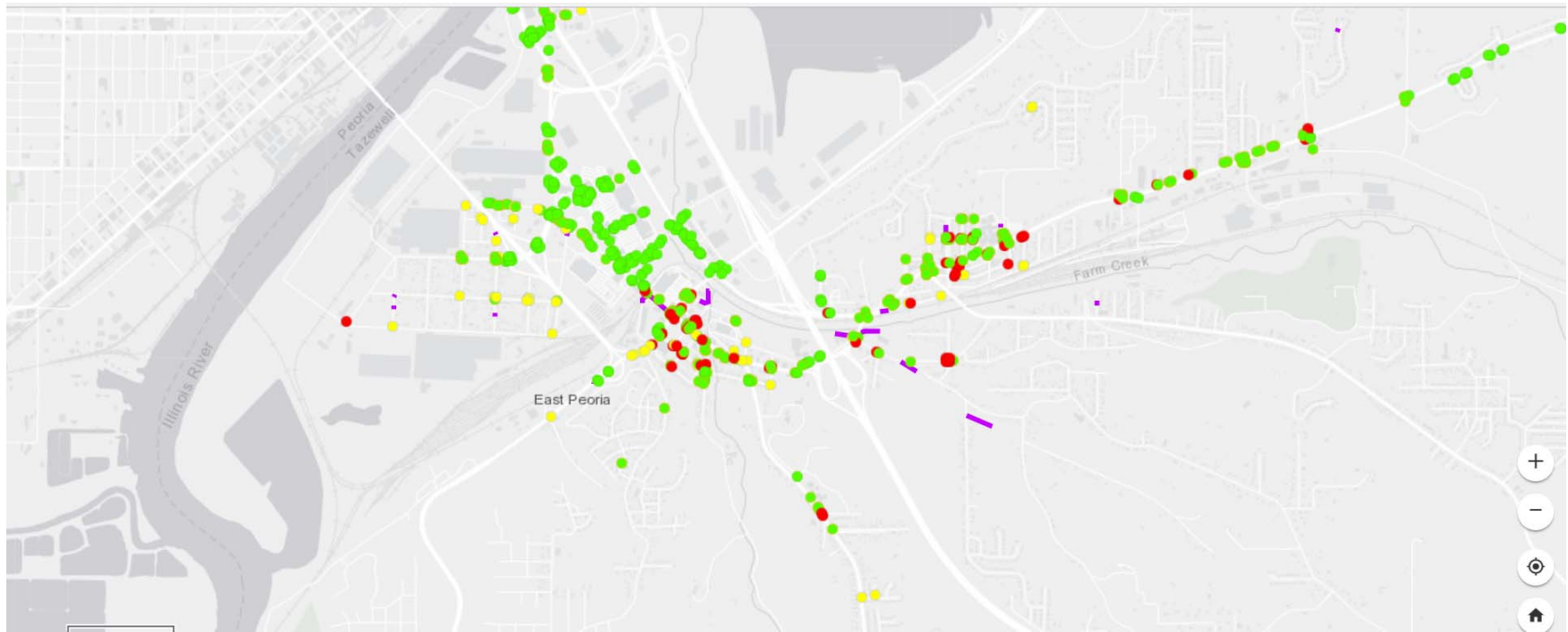
## **2.0(g) Monitor Progress**

*In order to be effective, the PROW accessibility transition plan needs to be utilized in planning and funding decisions. LPAs must update the PROW accessibility transition plan regularly to reflect changes in real world conditions and to address any possible new areas of noncompliance. Regular updates to the plan will also result in monitoring compliance and the effectiveness of priorities set in the plan itself.*

The ADA coordinator identified previously shall monitor progress of this accessibility transition plan.

## **Exhibit A**

## Map of Ramp Condition and Missing Sidewalk



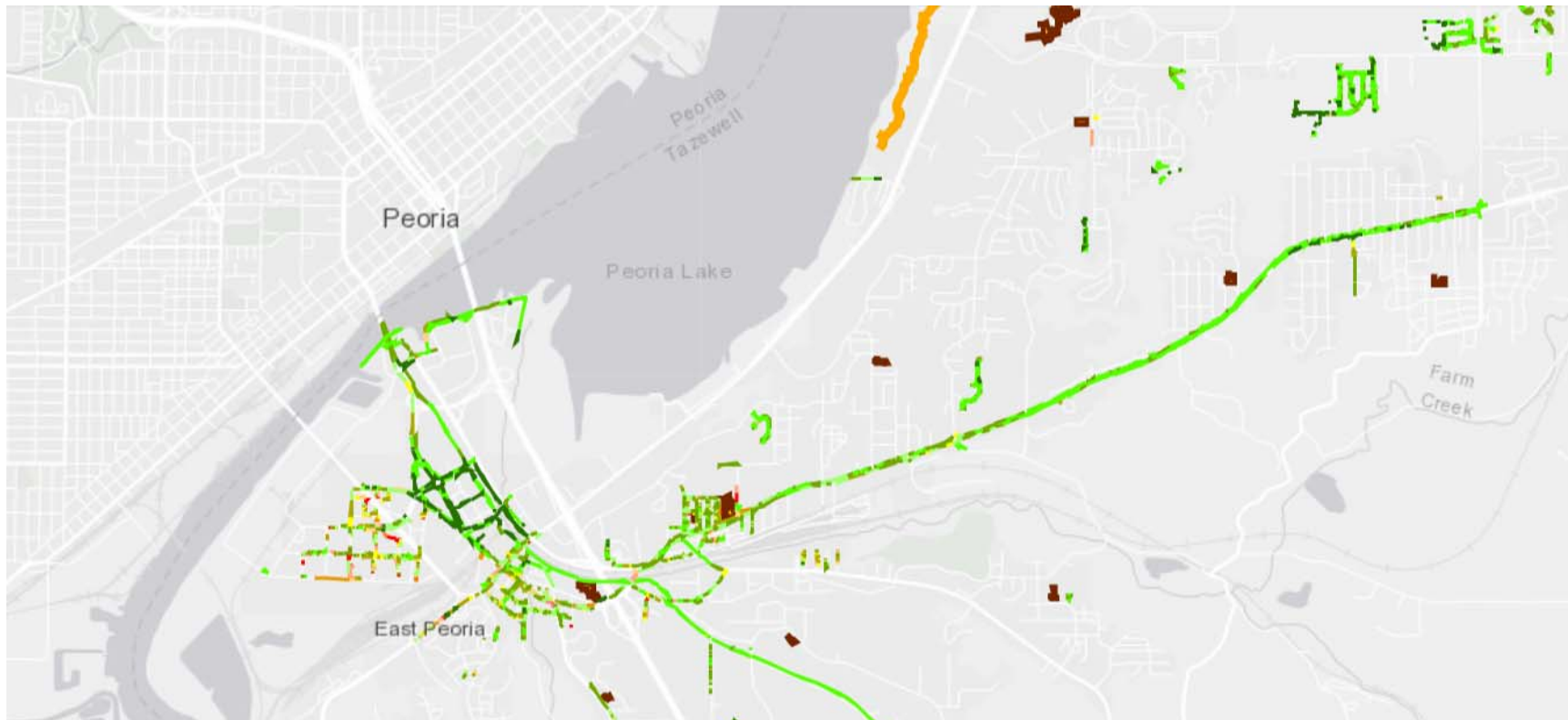
- ▼ ☒ Ramps
  - Unsafe Ramp: Deterioration, Excessive Slope
  - No ramp at pedestrian crossing in an area with sidewalks
  - Not feasible to build ramps due to slopes or obstacles
  - Safe ramps, good condition; but not fully compliant
  - Ramp in good condition, and fully compliant
  - All Other Ramps
- ▼ ☒ Missing Sidewalk Segments
  -

## Table of Ramp Condition

80%	559	=safe ramps but may not be fully compliant
20%	143	=total number of locations with no ramps or unsafe ramps (estimated at \$5,000 each is equivalent to approximately \$ 710,000
100%	702	=total number of ramps

702	375	3	14	4	34	10	58	143	61
Total # of Ramps	ADA w/ Domes	ADA w/ Domes	ADA w/ Domes	ADA w/out Domes	ADA w/out Domes	ADA w/out Domes	No Ramp w/ Sidewalk	Non-ADA	Non-ADA
	Ramp in good condition, and fully compliant	Safe ramps, good condition; but not fully compliant	Unsafe Ramp: Deterioration, Excessive Slope	Ramp in good condition, and fully compliant	Safe ramps, good condition; but not fully compliant	Unsafe Ramp: Deterioration, Excessive Slope	No ramp at pedestrian crossing in an area with sidewalks	Safe ramps, good condition; but not fully compliant	Unsafe Ramp: Deterioration, Excessive Slope

## Map of Sidewalk Condition



0% - Brand New/Near Perfect Sidewalk



10% - Very little Wear, Minor Spalling/Cracking



20% - Tripping Hazards / Rough Walking Surface



30% - Overlapping Spalling and Cracks / Vertical Lift



40% - 2/5th's Bad / Uneven Rough Surface / Tripping Hazards



50% - Half of sidewalk requires work or replacement



60% - Apparent Cracking, Vertical Lift, Spalling



70% - Unsafe Slope, Majority has Cracks and/or Slapping



80% - Some Sections are Completely Destroyed



90% - Nearly Entire Portion is Cracked or Spalled



100% - Entire Sidewalk is in Poor Condition and Unsafe



## Table of Sidewalk Condition

2%	45%	100% Bad Rating Condition - Entire Sidewalk is in Poor Condition and Unsafe
0%		90% Bad Rating Condition - Nearly Entire Portion is Cracked or Spalled
1%		80% Bad Rating Condition - Some Sections are Completely Destroyed
1%		70% Bad Rating Condition - Unsafe Slope, Majority has Cracks and/or Slapping
2%		60% Bad Rating Condition - Apparent Cracking, Vertical Lift, Spalling
2%		50% Bad Rating Condition - Half of sidewalk requires work or replacement
5%		40% Bad Rating Condition - 2/5th's Bad / Uneven Rough Surface / Tripping Hazards
9%		30% Bad Rating Condition - Overlapping Spalling and Cracks / Vertical Lift
22%		20% Bad Rating Condition - Tripping Hazards / Rough Walking Surface
41%	55%	10% Bad Rating Condition - Very little Wear, Minor Spalling/Cracking
14%		0% Bad Rating Condition - Brand New/Near Perfect Sidewalk

Vertical Lift

SidewalkID	Location	PIN	Vertical_Lift
58	215 CLARK ST	01-01-32-109-019	Extreme Lift - More than 2"
94	214 DIVISION ST	01-01-33-326-009	Extreme Lift - More than 2"
117	211 CLARK ST	01-01-32-109-003	Extreme Lift - More than 2"
151	212 DIVISION ST	01-01-33-326-008	Extreme Lift - More than 2"
226	207 CLARK ST	01-01-32-109-002	Extreme Lift - More than 2"
455	300 CHICAGO ST	01-01-32-302-012	Extreme Lift - More than 2"
509	203 CLARK ST	01-01-32-109-001	Extreme Lift - More than 2"
2182	112 SPRING ST	01-01-33-416-003	Extreme Lift - More than 2"
525	551 S MAIN ST	01-01-32-409-020	Extreme Lift - More than 2"
176	249 EDMUND ST	01-01-32-118-002	Extreme Lift - More than 2"
2232	100 WINTER CT	05-05-04-111-009	Extreme Lift - More than 2"
275	244 EDMUND ST	01-01-32-111-011	Extreme Lift - More than 2"
755	244 EDMUND ST	01-01-32-111-011	Extreme Lift - More than 2"
531	543 SANFORD ST	01-01-32-306-032	Extreme Lift - More than 2"
813	446 CHICAGO ST	01-01-32-301-003	Extreme Lift - More than 2"
848	203 CHICAGO ST	01-01-32-307-014	Extreme Lift - More than 2"
2108	114 W BLUFF ST	01-01-34-404-005	Extreme Lift - More than 2"
233	117-119 E WASHINGTON ST	01-01-33-305-007	Extreme Lift - More than 2"
2117	E CAMP ST	01-01-33-322-003	Extreme Lift - More than 2"
2137	E CAMP ST	01-01-33-322-003	Extreme Lift - More than 2"
424	105 ELM ST	01-01-32-111-012	Extreme Lift - More than 2"
2029	607 S PLEASANT HILL RD	05-05-12-112-012	Extreme Lift - More than 2"
2231	105 SEASON DR	05-05-04-111-006	Extreme Lift - More than 2"
2282	1109 SPRINGFIELD RD	05-05-04-111-002	Extreme Lift - More than 2"
126	112 GLOBE ST	01-01-32-106-009	Extreme Lift - More than 2"
174	215 GRANDVIEW AVE	01-01-32-419-003	Extreme Lift - More than 2"
278	127 STEWART ST	01-01-33-317-018	Extreme Lift - More than 2"
491	310 MAPLE ST	01-01-32-104-013	Extreme Lift - More than 2"
2215	148 SPRING ST	01-01-33-416-010	Extreme Lift - More than 2"
51	402 MONSON ST	01-01-32-117-010	Extreme Lift - More than 2"
78	305 EDMUND ST	01-01-32-117-009	Extreme Lift - More than 2"
148	W WASHINGTON ST	01-01-29-302-003	Extreme Lift - More than 2"
289	COLE ST	01-01-33-316-001	Extreme Lift - More than 2"
294	220 ALMIRON ST	01-01-33-314-014	Extreme Lift - More than 2"
444	258 EDMUND ST	01-01-32-111-010	Extreme Lift - More than 2"
728	108 GLOBE ST	01-01-32-106-008	Extreme Lift - More than 2"
1010	108 THOMAS CT	02-02-20-100-032	Extreme Lift - More than 2"
2127	146 SPRING ST	01-01-33-416-009	Extreme Lift - More than 2"
107	105 N MAIN ST	01-01-33-102-014	Extreme Lift - More than 2"
793	400 EDMUND ST	01-01-32-113-011	Extreme Lift - More than 2"
952	2540 E WASHINGTON ST	01-01-26-300-023	Extreme Lift - More than 2"
1602	141 PUTNAM ST	01-01-34-113-011	Extreme Lift - More than 2"
1872	E WASHINGTON ST	01-01-26-300-013	Extreme Lift - More than 2"
2061	100 KAITLIN CT	05-05-04-214-021	Extreme Lift - More than 2"
373	327 COLE ST	01-01-33-319-006	Extreme Lift - More than 2"
503	109-111 STEWART ST	01-01-33-317-014	Extreme Lift - More than 2"

SidewalkID	Location	PIN	Vertical_Lift
556	105 SANFORD ST	01-01-32-104-006	Extreme Lift - More than 2"
709	W WASHINGTON ST	01-01-29-302-003	Extreme Lift - More than 2"
828	101 RIVER RD	01-01-29-201-015	Extreme Lift - More than 2"
527	217 CHICAGO ST	01-01-32-307-010	Extreme Lift - More than 2"
324	601 RICHLAND ST	01-01-32-111-005	Extreme Lift - More than 2"
829	352 CENTER ST	01-01-32-401-028	Major Lift - 1"- 2"
1442	ROSEDALE RD	01-01-34-100-015	Major Lift - 1"- 2"
218	300 E WASHINGTON ST	01-01-33-317-011	Major Lift - 1"- 2"
548	615 S MAIN ST	01-01-32-409-016	Major Lift - 1"- 2"
1665	233 LEADLEY AVE	01-01-33-213-001	Major Lift - 1"- 2"
238	505 E WASHINGTON ST	01-01-33-324-001	Major Lift - 1"- 2"
1072	217 KEAYES AVE	01-01-33-407-017	Major Lift - 1"- 2"
1897	131 E SHEEN AVE	01-01-34-109-015	Major Lift - 1"- 2"
2281	1113 SPRINGFIELD RD	05-05-04-111-003	Major Lift - 1"- 2"
161	200 ANNA ST	01-01-33-303-006	Major Lift - 1"- 2"
647	200 S MAIN ST	01-01-32-405-017	Major Lift - 1"- 2"
989	ROSEDALE RD	01-01-34-100-015	Major Lift - 1"- 2"
2038	N/A	N/A	Major Lift - 1"- 2"
173	315 - 317 CHICAGO ST	01-01-32-306-008	Major Lift - 1"- 2"
193	204 ALMIRON ST	01-01-33-314-006	Major Lift - 1"- 2"
211	408 MONSON ST	01-01-32-117-011	Major Lift - 1"- 2"
401	227 EVERETT ST	01-01-33-313-016	Major Lift - 1"- 2"
408	215 ELM ST	01-01-32-114-011	Major Lift - 1"- 2"
443	531 E WASHINGTON ST	01-01-33-324-005	Major Lift - 1"- 2"
667	229 ELM ST	01-01-32-114-008	Major Lift - 1"- 2"
810	126 FLOYD ST	01-01-33-323-001	Major Lift - 1"- 2"
2	111 WASHINGTON ST	01-01-32-405-020	Major Lift - 1"- 2"
1568	ROSEDALE RD	01-01-34-105-001	Major Lift - 1"- 2"
434	RICHLAND ST	01-01-32-103-002	Major Lift - 1"- 2"
487	214 ALMIRON ST	01-01-33-314-011	Major Lift - 1"- 2"
563	304 1/2 CENTER ST	01-01-32-401-017	Major Lift - 1"- 2"
817	150 JOHNSON ST	01-01-32-413-012	Major Lift - 1"- 2"
851	340 CENTER ST	01-01-32-401-026	Major Lift - 1"- 2"
1757	402 KEAYES AVE	01-01-34-109-001	Major Lift - 1"- 2"
2039	601 S PLEASANT HILL RD	05-05-12-112-009	Major Lift - 1"- 2"
2069	307 CLOVER AVE	05-05-01-304-012	Major Lift - 1"- 2"
2100	108 W BLUFF ST	01-01-34-404-014	Major Lift - 1"- 2"
150	122 JOHNSON ST	01-01-32-413-006	Major Lift - 1"- 2"
395	114 JOHNSON ST	01-01-32-413-004	Major Lift - 1"- 2"
2151	E Washington St	01-01-33-508-002	Major Lift - 1"- 2"
65	101 GLOBE ST	01-01-32-107-005	Major Lift - 1"- 2"
68	226 CHICAGO ST	01-01-32-303-001	Major Lift - 1"- 2"
121	219 EVERETT ST	01-01-33-313-014	Major Lift - 1"- 2"
252	239 CHICAGO ST	01-01-32-307-006	Major Lift - 1"- 2"
367	521 MONSON ST	01-01-32-307-015	Major Lift - 1"- 2"
477	128 CENTER ST	01-01-32-202-010	Major Lift - 1"- 2"

SidewalkID	Location	PIN	Vertical_Lift
535	220 CHICAGO ST	01-01-32-303-003	Major Lift - 1"- 2"
891	104 CARLSON AVE	02-02-19-318-003	Major Lift - 1"- 2"
1221	141 PIERSON ST	01-01-33-209-001	Major Lift - 1"- 2"
1940	221 PERSHING PL	01-01-34-101-012	Major Lift - 1"- 2"
2102	133 KENWOOD AVE	01-01-34-401-011	Major Lift - 1"- 2"
36	324 EDMUND ST	01-01-32-114-020	Major Lift - 1"- 2"
188	309 WASHINGTON ST	01-01-33-323-007	Major Lift - 1"- 2"
378	145 JOHNSON ST	01-01-33-314-013	Major Lift - 1"- 2"
640	300 SPRINGFIELD RD	01-01-33-320-010	Major Lift - 1"- 2"
771	200 S MAIN ST	01-01-32-405-017	Major Lift - 1"- 2"
799			Major Lift - 1"- 2"
1099	FAIRLANE DR	01-01-14-101-004	Major Lift - 1"- 2"
1384	2601 E WASHINGTON ST	01-01-26-300-017	Major Lift - 1"- 2"
1542	450-2516 E WASHINGTON S	01-01-35-105-006	Major Lift - 1"- 2"
1617	233 LEADLEY AVE	01-01-33-213-001	Major Lift - 1"- 2"
2045	523 VETERANS DR	01-01-34-301-012	Major Lift - 1"- 2"
2469	108 FLOSS MAR CT	05-05-09-206-005	Major Lift - 1"- 2"
734	140 E WASHINGTON ST	01-01-33-312-004	Major Lift - 1"- 2"
22	116 GLOBE ST	01-01-32-106-010	Major Lift - 1"- 2"
80	102 GLOBE ST	01-01-32-106-006	Major Lift - 1"- 2"
152	114 STEWART ST	01-01-33-320-001	Major Lift - 1"- 2"
231	341 CHICAGO ST	01-01-32-306-003	Major Lift - 1"- 2"
335	141 CHICAGO ST	01-01-32-401-003	Major Lift - 1"- 2"
456	100 CHICAGO ST	01-01-32-400-011	Major Lift - 1"- 2"
457	213 CHICAGO ST	01-01-32-307-011	Major Lift - 1"- 2"
697	215 ALMIRON ST	01-01-33-315-032	Major Lift - 1"- 2"
710	420 MONSON ST	01-01-32-117-019	Major Lift - 1"- 2"
1326	219 & 219 1/2 E SHEEN AVE	01-01-34-109-019	Major Lift - 1"- 2"
1560	306 BARBARA PKWY	02-02-17-305-016	Major Lift - 1"- 2"
1932	106 SMILEY CT	02-02-17-302-033	Major Lift - 1"- 2"
2022	132 KENWOOD AVE	01-01-34-402-004	Major Lift - 1"- 2"
2086	509 S PLEASANT HILL RD	05-05-12-112-004	Major Lift - 1"- 2"
2153	152 SPRING ST	01-01-33-416-012	Major Lift - 1"- 2"
2170	611 S PLEASANT HILL RD	05-05-12-112-014	Major Lift - 1"- 2"
84			Major Lift - 1"- 2"
191	727 S MAIN ST	01-01-32-409-001	Major Lift - 1"- 2"
244	104 CLARK ST	01-01-32-104-010	Major Lift - 1"- 2"
321	155 E WASHINGTON ST	01-01-33-306-003	Major Lift - 1"- 2"
352	201 S MAIN ST	01-01-32-414-008	Major Lift - 1"- 2"
658	100 MONSON ST	01-01-32-105-005	Major Lift - 1"- 2"
706	325 EDMUND ST	01-01-32-117-007	Major Lift - 1"- 2"
805	355 EDMUND ST	01-01-32-117-002	Major Lift - 1"- 2"
814	154 E WASHINGTON ST	01-01-33-313-001	Major Lift - 1"- 2"
919	E WASHINGTON ST	01-01-34-105-009	Major Lift - 1"- 2"
1697	ROSEDALE RD	01-01-34-100-015	Major Lift - 1"- 2"
1732	119 PUTNAM ST	01-01-34-113-005	Major Lift - 1"- 2"

SidewalkID	Location	PIN	Vertical_Lift
2010	108 KAITLIN CT	05-05-04-214-019	Major Lift - 1"- 2"
2098	117 KAITLIN CT	05-05-04-214-013	Major Lift - 1"- 2"
2245	101 FLOSS MAR CT	05-05-09-205-011	Major Lift - 1"- 2"
40	220 EVERETT ST	01-01-33-315-007	Major Lift - 1"- 2"
92	146 CHICAGO ST	01-01-32-400-003	Major Lift - 1"- 2"
159	437 E WASHINGTON ST	01-01-33-323-022	Major Lift - 1"- 2"
212	333 CHICAGO ST	01-01-32-306-005	Major Lift - 1"- 2"
292	233 CHICAGO ST	01-01-32-307-008	Major Lift - 1"- 2"
372	100 N MAIN ST	01-01-33-302-014	Major Lift - 1"- 2"
520	501 RICHLAND ST	01-01-32-202-033	Major Lift - 1"- 2"
990	307 BARBARA PKWY	02-02-17-304-008	Major Lift - 1"- 2"
1057	112 SCENIC BLUFF DR	01-01-26-310-004	Major Lift - 1"- 2"
1124	215 PERSHING PL	01-01-34-101-013	Major Lift - 1"- 2"
1354	505 GEORGIA PKWY	02-02-17-311-012	Major Lift - 1"- 2"
1420	133 FLEUR DE LIS DR	01-01-27-312-030	Major Lift - 1"- 2"
1435	307 CARLOCK CT	02-02-17-305-008	Major Lift - 1"- 2"
1507	205 BARBARA PKWY	02-02-17-304-009	Major Lift - 1"- 2"
1512	106 SHANNON HILLS DR	02-02-18-404-013	Major Lift - 1"- 2"
1569	107 BROOKLYN CT	01-01-24-112-021	Major Lift - 1"- 2"
1625	201 ARNOLD AVE	01-01-33-208-010	Major Lift - 1"- 2"
1844	129 FLEUR DE LIS DR	01-01-27-312-031	Major Lift - 1"- 2"
2403	164 ELMRIDGE CIR	05-05-09-306-009	Major Lift - 1"- 2"
71	SPRINGFIELD RD	01-01-33-322-002	Major Lift - 1"- 2"
83	COLE ST	01-01-33-316-001	Major Lift - 1"- 2"
140	201 EVERETT ST	01-01-33-313-009	Major Lift - 1"- 2"
236	W WASHINGTON ST	01-01-29-302-003	Major Lift - 1"- 2"
247	333 EDMUND ST	01-01-32-117-006	Major Lift - 1"- 2"
271	521 E WASHINGTON ST	01-01-33-324-002	Major Lift - 1"- 2"
301	430 DOERING AVE	05-05-05-113-012	Major Lift - 1"- 2"
325	E CAMP ST	01-01-33-322-003	Major Lift - 1"- 2"
707	101 N MAIN ST	01-01-33-102-012	Major Lift - 1"- 2"
784			Major Lift - 1"- 2"
823	101 GLOBE ST	01-01-32-107-005	Major Lift - 1"- 2"
880	2421 WASHINGTON RD	02-02-19-318-001	Major Lift - 1"- 2"
1018	1303 E WASHINGTON ST	01-01-34-103-016	Major Lift - 1"- 2"
1213	1100 E WASHINGTON ST	01-01-33-216-003	Major Lift - 1"- 2"
1407	2001 E WASHINGTON ST	01-01-34-201-002	Major Lift - 1"- 2"
1476	E WASHINGTON ST	01-01-34-206-001	Major Lift - 1"- 2"
1497	1201 E WASHINGTON ST	01-01-33-213-008	Major Lift - 1"- 2"
1504	1605 E WASHINGTON ST	01-01-34-105-014	Major Lift - 1"- 2"
1609	407 JUNCTION AVE	01-01-34-108-003	Major Lift - 1"- 2"
1700	E WASHINGTON ST	01-01-34-201-001	Major Lift - 1"- 2"
2001	107 SUNNYBROOK DR	05-05-04-214-007	Major Lift - 1"- 2"
2056	109 KAITLIN CT	05-05-04-214-011	Major Lift - 1"- 2"
2074	221 SUNNYBROOK DR	05-05-04-214-024	Major Lift - 1"- 2"
286	327 CHICAGO ST	01-01-32-306-006	Major Lift - 1"- 2"

SidewalkID	Location	PIN	Vertical_Lift
620	357 COLE ST	01-01-33-319-012	Major Lift - 1"- 2"
772	225 JOHNSON ST	01-01-33-315-019	Major Lift - 1"- 2"
1014	113 RUTH CT	02-02-19-107-049	Major Lift - 1"- 2"
1041	511 JAMES PKWY	02-02-17-313-001	Major Lift - 1"- 2"
1108	206 BARBARA PKWY	02-02-17-305-035	Major Lift - 1"- 2"
1190	102 SKYLARK LN	02-02-18-407-005	Major Lift - 1"- 2"
1264	201 WHISTLING STRAIT	02-02-19-107-011	Major Lift - 1"- 2"
1301	101 THOMAS CT	02-02-20-100-025	Major Lift - 1"- 2"
1401	102 THOMAS CT	02-02-20-100-035	Major Lift - 1"- 2"
1491	104 MIROMAR LN	02-02-19-104-013	Major Lift - 1"- 2"
1918	FAIRLANE DR	01-01-14-101-011	Major Lift - 1"- 2"
2347	321 STONEWOOD DR	05-05-09-107-037	Major Lift - 1"- 2"
2358	105 STONE CREEK CT	05-05-09-107-004	Major Lift - 1"- 2"
2389	139 ELMRIDGE CIR	05-05-09-305-025	Major Lift - 1"- 2"
2397	121 ELMRIDGE CIR	05-05-09-305-006	Major Lift - 1"- 2"
2398	125 ELMRIDGE CIR	05-05-09-305-007	Major Lift - 1"- 2"
2399	129 ELMRIDGE CIR	05-05-09-305-008	Major Lift - 1"- 2"
2400	156 ELMRIDGE CIR	05-05-09-306-011	Major Lift - 1"- 2"
2468	141 ELMRIDGE CIR	05-05-09-305-023	Major Lift - 1"- 2"
356	105 N MAIN ST	01-01-33-102-014	Major Lift - 1"- 2"
391			Major Lift - 1"- 2"
458	W CAMP ST	01-01-33-100-031	Major Lift - 1"- 2"
753			Major Lift - 1"- 2"
1276	2530 E WASHINGTON ST	01-01-26-307-001	Major Lift - 1"- 2"
2033	213 SUNNYBROOK DR	05-05-04-214-023	Major Lift - 1"- 2"
519	1201 RIVERSIDE DR	01-01-29-201-010	Major Lift - 1"- 2"
342			Major Lift - 1"- 2"
2355	100 STONE CREEK CT	05-05-09-107-010	Major Lift - 1"- 2"
2471	329 STONEWOOD DR	05-05-09-107-041	Major Lift - 1"- 2"
749			Major Lift - 1"- 2"
842	W WASHINGTON ST	01-01-29-302-003	Major Lift - 1"- 2"
101	NCE CENTER DR (OFF OF)	01-01-29-300-016	Major Lift - 1"- 2"
493	128 FRANKLIN ST	01-01-32-118-019	Minor Lift - .25" - 1"
550	122 FRANKLIN ST	01-01-32-118-018	Minor Lift - .25" - 1"
629	122 FRANKLIN ST	01-01-32-118-018	Minor Lift - .25" - 1"
746	134 FRANKLIN ST	01-01-32-118-020	Minor Lift - .25" - 1"
291	412 MONSON ST	01-01-32-117-012	Minor Lift - .25" - 1"
344	328 CENTER ST	01-01-32-401-023	Minor Lift - .25" - 1"
1630	103 FLEUR DE LIS CT	01-01-27-312-017	Minor Lift - .25" - 1"
2148	305 CLOVER AVE	05-05-01-304-013	Minor Lift - .25" - 1"
552	555 S MAIN ST	01-01-32-409-019	Minor Lift - .25" - 1"
1245	ROSEDALE RD	01-01-34-100-015	Minor Lift - .25" - 1"
1944	1318 E WASHINGTON ST	01-01-34-109-005	Minor Lift - .25" - 1"
42	354 CENTER ST	01-01-32-401-029	Minor Lift - .25" - 1"
600	348 CENTER ST	01-01-32-401-027	Minor Lift - .25" - 1"
669	418 MONSON ST	01-01-32-117-018	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
833	249 EDMUND ST	01-01-32-118-002	Minor Lift - .25" - 1"
1996	137 KENWOOD AVE	01-01-34-401-010	Minor Lift - .25" - 1"
25	627 S MAIN ST	01-01-32-409-012	Minor Lift - .25" - 1"
714	617 S MAIN ST	01-01-32-409-014	Minor Lift - .25" - 1"
1251	1318 E WASHINGTON ST	01-01-34-109-005	Minor Lift - .25" - 1"
82	128 CENTER ST	01-01-32-202-009	Minor Lift - .25" - 1"
138	231 ELM ST	01-01-32-114-007	Minor Lift - .25" - 1"
195	223 ELM ST	01-01-32-114-009	Minor Lift - .25" - 1"
283	218 CLARK ST	01-01-32-108-005	Minor Lift - .25" - 1"
359	228 FRANKLIN ST	01-01-32-307-022	Minor Lift - .25" - 1"
365	227 CLARK ST	01-01-32-109-007	Minor Lift - .25" - 1"
719	526 PEKIN AVE	05-05-05-114-015	Minor Lift - .25" - 1"
780	117 CHICAGO ST	01-01-32-401-007	Minor Lift - .25" - 1"
1247	119 PIERSON ST	01-01-33-209-006	Minor Lift - .25" - 1"
2188	141 CATHERINE ST	01-01-34-400-002	Minor Lift - .25" - 1"
2283	1105 SPRINGFIELD RD	05-05-04-111-001	Minor Lift - .25" - 1"
81	140 E WASHINGTON ST	01-01-33-312-002	Minor Lift - .25" - 1"
314	607 S MAIN ST	01-01-32-409-017	Minor Lift - .25" - 1"
608	653 S MAIN ST	01-01-32-409-005	Minor Lift - .25" - 1"
613	E WASHINGTON ST	01-01-33-306-005	Minor Lift - .25" - 1"
743	213 E WASHINGTON ST	01-01-33-311-014	Minor Lift - .25" - 1"
1075	318 ARNOLD AVE	01-01-33-212-004	Minor Lift - .25" - 1"
1346	202 E SHEEN AVE	01-01-34-111-013	Minor Lift - .25" - 1"
2311	107 FLOSS MAR CT	05-05-09-205-014	Minor Lift - .25" - 1"
479	RIVER RD	01-01-29-201-021	Minor Lift - .25" - 1"
67	140 FRANKLIN ST	01-01-32-118-021	Minor Lift - .25" - 1"
90	233 ELM ST	01-01-32-114-006	Minor Lift - .25" - 1"
133	339 CHICAGO ST	01-01-32-306-004	Minor Lift - .25" - 1"
179	222 E WASHINGTON ST	01-01-33-317-002	Minor Lift - .25" - 1"
190	151 STEWART ST	01-01-33-317-023	Minor Lift - .25" - 1"
263	324 CENTER ST	01-01-32-401-022	Minor Lift - .25" - 1"
304	243 CHICAGO ST	01-01-32-307-005	Minor Lift - .25" - 1"
374	105 ELM ST	01-01-32-111-012	Minor Lift - .25" - 1"
417	143 FRANKLIN ST	01-01-32-203-016	Minor Lift - .25" - 1"
446	152 JOHNSON ST	01-01-32-413-013	Minor Lift - .25" - 1"
459	253 CHICAGO ST	01-01-32-307-001	Minor Lift - .25" - 1"
474	202 CHICAGO ST	01-01-32-303-012	Minor Lift - .25" - 1"
478	201 JOHNSON ST	01-01-33-315-031	Minor Lift - .25" - 1"
498	206 CHICAGO ST	01-01-32-303-010	Minor Lift - .25" - 1"
588	222 EVERETT ST	01-01-33-315-008	Minor Lift - .25" - 1"
589	254 JOHNSON ST	01-01-33-318-023	Minor Lift - .25" - 1"
611	523 MONSON ST	01-01-32-307-016	Minor Lift - .25" - 1"
633	223 EVERETT ST	01-01-33-313-015	Minor Lift - .25" - 1"
642	126 STEWART ST	01-01-33-320-004	Minor Lift - .25" - 1"
646	139 STEWART ST	01-01-33-317-020	Minor Lift - .25" - 1"
682	320 CHICAGO ST	01-01-32-302-007	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
712	310 CHICAGO ST	01-01-32-302-009	Minor Lift - .25" - 1"
744	139 CHICAGO ST	01-01-32-401-004	Minor Lift - .25" - 1"
754	159 STEWART ST	01-01-33-317-025	Minor Lift - .25" - 1"
767	124 CENTER ST	01-01-32-202-008	Minor Lift - .25" - 1"
1180	121 HARDING BLVD	01-01-34-104-006	Minor Lift - .25" - 1"
1283	132 PUTNAM ST	01-01-34-110-015	Minor Lift - .25" - 1"
1509	117 HARDING BLVD	01-01-34-104-007	Minor Lift - .25" - 1"
1833	234 PERSHING PL	01-01-34-102-001	Minor Lift - .25" - 1"
1984	118 CATHERINE ST	01-01-34-401-006	Minor Lift - .25" - 1"
1985	120 CATHERINE ST	01-01-34-401-005	Minor Lift - .25" - 1"
1990	102 KENWOOD AVE	01-01-34-402-009	Minor Lift - .25" - 1"
2021	114 KENWOOD AVE	01-01-34-402-008	Minor Lift - .25" - 1"
2024	162 SPRING ST	01-01-33-416-014	Minor Lift - .25" - 1"
2126	129 CATHERINE ST	01-01-34-400-004	Minor Lift - .25" - 1"
2132	132 CATHERINE ST	01-01-34-401-002	Minor Lift - .25" - 1"
2160	166 SPRING ST	01-01-33-416-015	Minor Lift - .25" - 1"
2175	517 S PLEASANT HILL RD	05-05-12-112-008	Minor Lift - .25" - 1"
2186	125 KENWOOD AVE	01-01-34-401-013	Minor Lift - .25" - 1"
2214	122 KENWOOD AVE	01-01-34-402-006	Minor Lift - .25" - 1"
45	359 EDMUND ST	01-01-32-117-001	Minor Lift - .25" - 1"
185	430 PROSPECT AVE	01-01-32-409-004	Minor Lift - .25" - 1"
398	123 STEWART ST	01-01-33-317-017	Minor Lift - .25" - 1"
413	139 E WASHINGTON ST	01-01-33-306-004	Minor Lift - .25" - 1"
539	129 E WASHINGTON ST	01-01-33-305-008	Minor Lift - .25" - 1"
587	633 S MAIN ST	01-01-32-409-010	Minor Lift - .25" - 1"
680			Minor Lift - .25" - 1"
844	323 EDMUND ST	01-01-32-117-008	Minor Lift - .25" - 1"
961	819 E CAMP ST	01-01-33-401-014	Minor Lift - .25" - 1"
1179	137 PUTNAM ST	01-01-34-113-010	Minor Lift - .25" - 1"
1631	8 RONALD CT	02-02-17-311-020	Minor Lift - .25" - 1"
1910	7 RONALD CT	02-02-17-311-016	Minor Lift - .25" - 1"
8	217- 221 FISHER ST	01-01-32-304-001	Minor Lift - .25" - 1"
53	146 JOHNSON ST	01-01-32-413-011	Minor Lift - .25" - 1"
59	207 CHICAGO ST	01-01-32-307-013	Minor Lift - .25" - 1"
123	211 GRANDVIEW AVE	01-01-32-419-002	Minor Lift - .25" - 1"
194	257 JOHNSON ST	01-01-33-315-029	Minor Lift - .25" - 1"
200	251 CHICAGO ST	01-01-32-307-003	Minor Lift - .25" - 1"
242	214 COLE ST	01-01-33-315-030	Minor Lift - .25" - 1"
282	323 CHICAGO ST	01-01-32-306-007	Minor Lift - .25" - 1"
307	216 CLARK ST	01-01-32-108-004	Minor Lift - .25" - 1"
313	246 JOHNSON ST	01-01-33-318-020	Minor Lift - .25" - 1"
383	262 JOHNSON ST	01-01-33-318-040	Minor Lift - .25" - 1"
400	316 CENTER ST	01-01-32-401-021	Minor Lift - .25" - 1"
453	336 CENTER ST	01-01-32-401-025	Minor Lift - .25" - 1"
551	330 EDMUND ST	01-01-32-114-019	Minor Lift - .25" - 1"
557	260 JOHNSON ST	01-01-33-318-042	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
573	347 COLE ST	01-01-33-319-010	Minor Lift - .25" - 1"
619	120 CENTER ST	01-01-32-202-007	Minor Lift - .25" - 1"
625	429 E WASHINGTON ST	01-01-33-323-016	Minor Lift - .25" - 1"
663	212 CHICAGO ST	01-01-32-303-007	Minor Lift - .25" - 1"
683	224 EVERETT ST	01-01-33-315-009	Minor Lift - .25" - 1"
691	235 CHICAGO ST	01-01-32-307-007	Minor Lift - .25" - 1"
693	111 WASHINGTON ST	01-01-32-405-020	Minor Lift - .25" - 1"
758	230 JOHNSON ST	01-01-33-318-007	Minor Lift - .25" - 1"
794	108 MONSON ST	01-01-32-105-008	Minor Lift - .25" - 1"
802	353 COLE ST	01-01-33-319-011	Minor Lift - .25" - 1"
804	100 CHICAGO ST	01-01-32-400-011	Minor Lift - .25" - 1"
824	236 MONSON ST	01-01-32-109-018	Minor Lift - .25" - 1"
935	136 PUTNAM ST	01-01-34-110-016	Minor Lift - .25" - 1"
1130	102 BROOKLYN CT	01-01-24-112-035	Minor Lift - .25" - 1"
1156	604 GEORGIA PKWY	02-02-17-309-014	Minor Lift - .25" - 1"
1197	114 ARNOLD AVE	01-01-33-213-006	Minor Lift - .25" - 1"
1382	FAIRLANE DR	01-01-14-101-005	Minor Lift - .25" - 1"
1452	209 KEAYES AVE	01-01-33-407-013	Minor Lift - .25" - 1"
1547	1343 E WASHINGTON ST	01-01-34-104-010	Minor Lift - .25" - 1"
1565	138 E SHEEN AVE	01-01-34-111-017	Minor Lift - .25" - 1"
1707	308 CARLOCK CT	02-02-17-305-028	Minor Lift - .25" - 1"
1712	128 PUTNAM ST	01-01-34-110-014	Minor Lift - .25" - 1"
1845	306 CARLOCK CT	02-02-17-305-029	Minor Lift - .25" - 1"
1913	100 BROOKLYN CT	01-01-24-112-016	Minor Lift - .25" - 1"
1953	405 WOODROW DR	01-01-33-206-029	Minor Lift - .25" - 1"
2015	158 SPRING ST	01-01-33-416-013	Minor Lift - .25" - 1"
2066	423 VETERANS DR	01-01-33-423-002	Minor Lift - .25" - 1"
2114	134 CATHERINE ST	01-01-34-401-001	Minor Lift - .25" - 1"
2150	701 MEADOW AVE	01-01-34-403-020	Minor Lift - .25" - 1"
2163	120 KENWOOD AVE	01-01-34-402-007	Minor Lift - .25" - 1"
2184	133 CATHERINE ST	01-01-34-400-003	Minor Lift - .25" - 1"
2185	112 CALLENDER AVE	01-01-34-403-009	Minor Lift - .25" - 1"
91	312 EDMUND ST	01-01-32-114-021	Minor Lift - .25" - 1"
111			Minor Lift - .25" - 1"
169	141 JOHNSON ST	01-01-33-314-008	Minor Lift - .25" - 1"
251	105 N MAIN ST	01-01-33-102-014	Minor Lift - .25" - 1"
256	401 S MAIN ST	01-01-32-412-003	Minor Lift - .25" - 1"
380	444 E WASHINGTON ST	01-01-33-325-017	Minor Lift - .25" - 1"
438	106 COLE ST	01-01-33-313-003	Minor Lift - .25" - 1"
485	W WASHINGTON ST	01-01-32-103-004	Minor Lift - .25" - 1"
495	103 N MAIN ST	01-01-33-102-015	Minor Lift - .25" - 1"
521	57 - 259 E WASHINGTON S	01-01-33-311-010	Minor Lift - .25" - 1"
703	101 MONSON ST	01-01-32-106-001	Minor Lift - .25" - 1"
733	263 E WASHINGTON ST	01-01-33-311-011	Minor Lift - .25" - 1"
781	310 EDMOND ST	01-01-32-114-022	Minor Lift - .25" - 1"
825	207 E WASHINGTON ST	01-01-33-311-003	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
886	110 CARLSON AVE	02-02-19-320-002	Minor Lift - .25" - 1"
887	108 CARLSON AVE	02-02-19-320-001	Minor Lift - .25" - 1"
951	2200 E WASHINGTON ST	01-01-34-205-003	Minor Lift - .25" - 1"
966	1303 E WASHINGTON ST	01-01-34-103-016	Minor Lift - .25" - 1"
986	320 LEADLEY AVE	01-01-34-101-004	Minor Lift - .25" - 1"
1067	129 CAROLINE ST	01-01-34-114-033	Minor Lift - .25" - 1"
1129	140 CAROLINE ST	01-01-34-113-021	Minor Lift - .25" - 1"
1310	1621 E WASHINGTON ST	01-01-34-105-005	Minor Lift - .25" - 1"
1347	2200 E WASHINGTON ST	01-01-34-205-003	Minor Lift - .25" - 1"
1468	N/A	NA	Minor Lift - .25" - 1"
1520	5 RONALD CT	02-02-17-311-015	Minor Lift - .25" - 1"
1587	118 CAROLINE ST	01-01-34-113-016	Minor Lift - .25" - 1"
1604	142 CAROLINE ST	01-01-34-113-022	Minor Lift - .25" - 1"
1605	215 FAIRVIEW AVE	01-01-33-400-001	Minor Lift - .25" - 1"
1760	N/A	NA	Minor Lift - .25" - 1"
1817	312 ARNOLD AVE	01-01-33-212-006	Minor Lift - .25" - 1"
1887	1343 E WASHINGTON ST	01-01-34-104-010	Minor Lift - .25" - 1"
2028	137 KERFOOT ST	05-05-04-200-018	Minor Lift - .25" - 1"
2103	208 SUNNYBROOK DR	05-05-04-216-024	Minor Lift - .25" - 1"
2310	105 FLOSS MAR CT	05-05-09-205-013	Minor Lift - .25" - 1"
21	100 MONSON ST	01-01-32-105-005	Minor Lift - .25" - 1"
52	208 EVERETT ST	01-01-33-315-004	Minor Lift - .25" - 1"
97			Minor Lift - .25" - 1"
105	216 ALMIRON ST	01-01-33-314-012	Minor Lift - .25" - 1"
116	218 CHICAGO ST	01-01-32-303-004	Minor Lift - .25" - 1"
130	RICHLAND ST	01-01-32-103-002	Minor Lift - .25" - 1"
132	127 SPRINGFIELD RD	01-01-33-325-011	Minor Lift - .25" - 1"
139	212 JOHNSON ST	01-01-33-318-003	Minor Lift - .25" - 1"
157	239 JOHNSON ST	01-01-33-315-022	Minor Lift - .25" - 1"
177	147 FRANKLIN ST	01-01-32-400-001	Minor Lift - .25" - 1"
222	417 MONSON ST	01-01-32-118-011	Minor Lift - .25" - 1"
249	132 STEWART ST	01-01-33-320-006	Minor Lift - .25" - 1"
269	137 FRANKLIN ST	01-01-32-203-015	Minor Lift - .25" - 1"
297	230 EVERETT ST	01-01-33-315-011	Minor Lift - .25" - 1"
298	201 COLE ST	01-01-33-319-001	Minor Lift - .25" - 1"
308	228 EVERETT ST	01-01-33-315-010	Minor Lift - .25" - 1"
309	232 E WASHINGTON ST	01-01-33-317-003	Minor Lift - .25" - 1"
322	520 MONSON ST	01-01-32-306-016	Minor Lift - .25" - 1"
347			Minor Lift - .25" - 1"
467	301 COLE ST	01-01-33-319-004	Minor Lift - .25" - 1"
475	425 MONSON ST	01-01-32-118-012	Minor Lift - .25" - 1"
488	434 MONSON ST	01-01-32-117-021	Minor Lift - .25" - 1"
496	314 CHICAGO ST	01-01-32-302-008	Minor Lift - .25" - 1"
553	125 CHICAGO ST	01-01-32-401-005	Minor Lift - .25" - 1"
594	235 CLARK ST	01-01-32-109-009	Minor Lift - .25" - 1"
636	100 N MAIN ST	01-01-33-302-014	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
637	323 COLE ST	01-01-33-319-005	Minor Lift - .25" - 1"
651	200 COLE ST	01-01-33-315-024	Minor Lift - .25" - 1"
655	210 CHICAGO ST	01-01-32-303-008	Minor Lift - .25" - 1"
664	266 E WASHINGTON ST	01-01-33-317-010	Minor Lift - .25" - 1"
675	216 CHICAGO ST	01-01-32-303-005	Minor Lift - .25" - 1"
751	214 CHICAGO ST	01-01-32-303-006	Minor Lift - .25" - 1"
768	204 CHICAGO ST	01-01-32-303-011	Minor Lift - .25" - 1"
895	FAIRLANE DR	01-01-14-101-014	Minor Lift - .25" - 1"
904	605 GEORGIA PKWY	02-02-17-311-013	Minor Lift - .25" - 1"
908	122 WOODROW DR	01-01-34-100-006	Minor Lift - .25" - 1"
963	601 GEORGIA PKWY	02-02-17-311-010	Minor Lift - .25" - 1"
978	217 BARBARA PKWY	02-02-17-304-004	Minor Lift - .25" - 1"
1009	105 SMILEY CT	02-02-17-302-018	Minor Lift - .25" - 1"
1031	225 ARNOLD AVE	01-01-33-206-043	Minor Lift - .25" - 1"
1062	FAIRLANE DR	01-01-14-101-007	Minor Lift - .25" - 1"
1140	401 WOODROW DR	01-01-33-206-031	Minor Lift - .25" - 1"
1200	209 ARNOLD AVE	01-01-33-206-047	Minor Lift - .25" - 1"
1304	100 FLEUR DE LIS CT	01-01-27-312-012	Minor Lift - .25" - 1"
1328	104 BROOKLYN CT	01-01-24-112-032	Minor Lift - .25" - 1"
1329	315 ARNOLD AVE	01-01-33-206-038	Minor Lift - .25" - 1"
1352	115 SHADOW CREEK	02-02-19-102-013	Minor Lift - .25" - 1"
1363	504 JAMES PKWY	02-02-17-311-024	Minor Lift - .25" - 1"
1369	105 MIROMAR LN	02-02-19-105-003	Minor Lift - .25" - 1"
1388	201 HARDING BLVD	01-01-34-102-017	Minor Lift - .25" - 1"
1389	111 ARNOLD AVE	01-01-33-209-009	Minor Lift - .25" - 1"
1392	3 RONALD CT	02-02-17-311-014	Minor Lift - .25" - 1"
1395	2416 WASHINGTON RD	02-02-19-312-022	Minor Lift - .25" - 1"
1436	220 PERSHING PL	01-01-34-102-004	Minor Lift - .25" - 1"
1439	130 PIERSON ST	01-01-33-208-001	Minor Lift - .25" - 1"
1449	305 BARBARA PKWY	02-02-17-304-006	Minor Lift - .25" - 1"
1489	218 BARBARA PKWY	02-02-17-305-021	Minor Lift - .25" - 1"
1549	106 THOMAS CT	02-02-20-100-033	Minor Lift - .25" - 1"
1594	205 KEAYES AVE	01-01-33-407-011	Minor Lift - .25" - 1"
1642	213 KEAYES AVE	01-01-33-407-015	Minor Lift - .25" - 1"
1666	203 HARDING BLVD	01-01-34-102-016	Minor Lift - .25" - 1"
1688	N MAIN ST (OFF OF)	01-01-22-200-004	Minor Lift - .25" - 1"
1711	115 HARDING BLVD	01-01-34-104-008	Minor Lift - .25" - 1"
1748	325 ARNOLD AVE	01-01-33-206-036	Minor Lift - .25" - 1"
1758	106 BROOKLYN CT	01-01-24-112-019	Minor Lift - .25" - 1"
1815	347 ARNOLD AVE	01-01-33-206-032	Minor Lift - .25" - 1"
1867	KEAYES AVE	01-01-34-300-001	Minor Lift - .25" - 1"
1875	127 CAROLINE ST	01-01-34-114-011	Minor Lift - .25" - 1"
1881	207 E SHEEN AVE	01-01-34-109-018	Minor Lift - .25" - 1"
1899	606 GEORGIA PKWY	02-02-17-309-015	Minor Lift - .25" - 1"
1921	110 PIERSON ST	01-01-33-208-008	Minor Lift - .25" - 1"
1947	139 PERSHING PL	01-01-34-103-009	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1974	205 HARDING BLVD	01-01-34-102-015	Minor Lift - .25" - 1"
1982	113 SMILEY CT	02-02-17-305-010	Minor Lift - .25" - 1"
1988	138 SPRING ST	01-01-33-416-008	Minor Lift - .25" - 1"
2009	134 SPRING ST	01-01-33-416-007	Minor Lift - .25" - 1"
2121	201 HELENE CT	05-05-02-418-039	Minor Lift - .25" - 1"
2405	144 ELMRIDGE CIR	05-05-09-306-015	Minor Lift - .25" - 1"
2407	104 ELMRIDGE CIR	05-05-09-306-002	Minor Lift - .25" - 1"
2411	120 ELMRIDGE CIR	05-05-09-306-006	Minor Lift - .25" - 1"
37	201 S MAIN ST	01-01-32-414-008	Minor Lift - .25" - 1"
62	W WASHINGTON ST	01-01-29-302-003	Minor Lift - .25" - 1"
73	SPRINGFIELD RD	01-01-33-308-011	Minor Lift - .25" - 1"
166	351 EDMUND ST	01-01-32-117-003	Minor Lift - .25" - 1"
205	252 E WASHINGTON ST	01-01-33-317-009	Minor Lift - .25" - 1"
209	102 GLOBE ST	01-01-32-106-006	Minor Lift - .25" - 1"
253			Minor Lift - .25" - 1"
302	300 N MAIN ST	01-01-33-103-004	Minor Lift - .25" - 1"
362	W WASHINGTON ST	01-01-29-302-003	Minor Lift - .25" - 1"
489	727 S MAIN ST	01-01-32-409-001	Minor Lift - .25" - 1"
513	107 E WASHINGTON ST	01-01-33-305-003	Minor Lift - .25" - 1"
541	RENCE CENTER DR (PARK)	01-01-29-300-011	Minor Lift - .25" - 1"
565	338 COLE ST	01-01-33-318-031	Minor Lift - .25" - 1"
575			Minor Lift - .25" - 1"
597	E WASHINGTON ST	01-01-33-311-001	Minor Lift - .25" - 1"
617	222 ANNA ST	01-01-33-303-001	Minor Lift - .25" - 1"
689	300 SPRINGFIELD RD	01-01-33-320-010	Minor Lift - .25" - 1"
702			Minor Lift - .25" - 1"
740	140 N MAIN ST	01-01-33-302-001	Minor Lift - .25" - 1"
742	418 DOERING AVE	05-05-05-113-009	Minor Lift - .25" - 1"
787	214 ANNA ST	01-01-33-303-003	Minor Lift - .25" - 1"
791	101 N MAIN ST	01-01-33-102-012	Minor Lift - .25" - 1"
812	201 S MAIN ST	01-01-32-414-008	Minor Lift - .25" - 1"
884	102 DONALD CT	01-01-26-406-009	Minor Lift - .25" - 1"
902	1954 E WASHINGTON ST	01-01-34-204-041	Minor Lift - .25" - 1"
1060	101 FARMDALE RD	01-01-26-300-028	Minor Lift - .25" - 1"
1068	1986 E WASHINGTON ST	01-01-34-204-030	Minor Lift - .25" - 1"
1145	1100 E WASHINGTON ST	01-01-33-216-001	Minor Lift - .25" - 1"
1146	146 CAROLINE ST	01-01-34-113-023	Minor Lift - .25" - 1"
1164	1705 E WASHINGTON ST	01-01-34-105-007	Minor Lift - .25" - 1"
1170	N/A	NA	Minor Lift - .25" - 1"
1174	N/A	N/A	Minor Lift - .25" - 1"
1226	E WASHINGTON ST	01-01-26-300-039	Minor Lift - .25" - 1"
1267	256 LEADLEY AVE	01-01-34-103-003	Minor Lift - .25" - 1"
1286	ROSEDALE RD	01-01-34-100-015	Minor Lift - .25" - 1"
1294	709 E CAMP ST	01-01-33-401-010	Minor Lift - .25" - 1"
1315	311 WOODROW DR	01-01-33-212-001	Minor Lift - .25" - 1"
1365	2321 E WASHINGTON ST	01-01-35-100-006	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1393	1701 E WASHINGTON ST	01-01-34-105-006	Minor Lift - .25" - 1"
1415	150-2516 E WASHINGTON ST	01-01-35-105-006	Minor Lift - .25" - 1"
1432	1404 E WASHINGTON ST	01-01-34-109-009	Minor Lift - .25" - 1"
1498	E WASHINGTON ST	01-01-26-300-039	Minor Lift - .25" - 1"
1584	1350 E WASHINGTON ST	01-01-34-109-007	Minor Lift - .25" - 1"
1585	262 LEADLEY AVE	01-01-34-103-001	Minor Lift - .25" - 1"
1611	1721 E WASHINGTON ST	01-01-34-105-010	Minor Lift - .25" - 1"
1628	2400 E WASHINGTON ST	01-01-35-105-004	Minor Lift - .25" - 1"
1653	107 PUTNAM ST	01-01-34-113-004	Minor Lift - .25" - 1"
1677	1343 E WASHINGTON ST	01-01-34-104-010	Minor Lift - .25" - 1"
1704	121 LEADLEY AVE	01-01-33-214-004	Minor Lift - .25" - 1"
1754	N/A	N/A	Minor Lift - .25" - 1"
1762	229 FAIRVIEW AVE	01-01-33-202-005	Minor Lift - .25" - 1"
1841	1414 E WASHINGTON ST	01-01-34-109-012	Minor Lift - .25" - 1"
1878	927 E WASHINGTON ST	01-01-33-402-014	Minor Lift - .25" - 1"
1883	128 CAROLINE ST	01-01-34-113-020	Minor Lift - .25" - 1"
1901	112 ARNOLD AVE	01-01-33-213-007	Minor Lift - .25" - 1"
1929	154 CAROLINE ST	01-01-34-113-025	Minor Lift - .25" - 1"
1956	1715 E WASHINGTON ST	01-01-34-105-008	Minor Lift - .25" - 1"
2032	101 KAREN DR	05-05-04-215-005	Minor Lift - .25" - 1"
2048	205 KERFOOT ST	05-05-04-200-022	Minor Lift - .25" - 1"
2053	435 VETERANS DR	01-01-34-301-018	Minor Lift - .25" - 1"
2087	120 SUNNYBROOK DR	05-05-04-216-021	Minor Lift - .25" - 1"
2107	105 KAREN DR	05-05-04-215-004	Minor Lift - .25" - 1"
2120	116 SUNNYBROOK DR	05-05-04-216-020	Minor Lift - .25" - 1"
2138	113 SUNNYBROOK DR	05-05-04-214-008	Minor Lift - .25" - 1"
2192	E Camp St	01-01-33-508-003	Minor Lift - .25" - 1"
2203	113 KERFOOT ST	05-05-04-200-007	Minor Lift - .25" - 1"
2217	407 VETERANS DR	01-01-34-301-022	Minor Lift - .25" - 1"
2221	800 E WASHINGTON ST	01-01-33-412-003	Minor Lift - .25" - 1"
2313	106 FLOSS MAR CT	05-05-09-206-004	Minor Lift - .25" - 1"
6	228 MONSON ST	01-01-32-109-016	Minor Lift - .25" - 1"
628	MATHENY RD (OFF OF)	05-05-03-213-008	Minor Lift - .25" - 1"
70	229 JOHNSON ST	01-01-33-315-020	Minor Lift - .25" - 1"
93	252 E WASHINGTON ST	01-01-33-317-008	Minor Lift - .25" - 1"
115	251 JOHNSON ST	01-01-33-315-023	Minor Lift - .25" - 1"
183	552 CASS ST	01-01-32-304-022	Minor Lift - .25" - 1"
187	146 STEWART ST	01-01-33-320-008	Minor Lift - .25" - 1"
203	312 PEKIN AVE	01-01-32-419-009	Minor Lift - .25" - 1"
204	448 MONSON ST	01-01-32-117-022	Minor Lift - .25" - 1"
245	725 TAYLOR ST	01-01-33-409-014	Minor Lift - .25" - 1"
288			Minor Lift - .25" - 1"
303	229 SPRINGFIELD RD	01-01-33-326-010	Minor Lift - .25" - 1"
337	109 E WASHINGTON ST	01-01-33-305-004	Minor Lift - .25" - 1"
371	212 EVERETT ST	01-01-33-315-006	Minor Lift - .25" - 1"
381	223 CHICAGO ST	01-01-32-307-009	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
448	141 STEWART ST	01-01-33-317-028	Minor Lift - .25" - 1"
469	122 CHICAGO ST	01-01-32-400-006	Minor Lift - .25" - 1"
584	115 STEWART ST	01-01-33-317-015	Minor Lift - .25" - 1"
599	117 STEWART ST	01-01-33-317-016	Minor Lift - .25" - 1"
604	W WASHINGTON ST	01-01-29-302-003	Minor Lift - .25" - 1"
607	422 E WASHINGTON ST	01-01-33-325-006	Minor Lift - .25" - 1"
614	208 CHICAGO ST	01-01-32-303-009	Minor Lift - .25" - 1"
639	1 CONFERENCE CENTER	01-01-29-401-035	Minor Lift - .25" - 1"
672	213 JOHNSON ST	01-01-33-315-016	Minor Lift - .25" - 1"
705	ALTORFER LN (ROW)	01-01-33-100-032	Minor Lift - .25" - 1"
721	217 JOHNSON ST	01-01-33-315-017	Minor Lift - .25" - 1"
732	308 CHICAGO ST	01-01-32-302-010	Minor Lift - .25" - 1"
738	155 STEWART ST	01-01-33-317-024	Minor Lift - .25" - 1"
779	237 FRANKLIN ST	01-01-32-401-012	Minor Lift - .25" - 1"
815	235 JOHNSON ST	01-01-33-315-021	Minor Lift - .25" - 1"
881	200 CARLSON AVE	02-02-30-118-015	Minor Lift - .25" - 1"
889	200 CARLSON AVE	02-02-30-118-004	Minor Lift - .25" - 1"
898	140 FLEUR DE LIS DR	01-01-27-312-025	Minor Lift - .25" - 1"
906	149 FAWN HAVEN DR	01-01-26-308-033	Minor Lift - .25" - 1"
910	131 PIERSON ST	01-01-33-209-002	Minor Lift - .25" - 1"
930	104 MADISSYN CT	01-01-24-112-003	Minor Lift - .25" - 1"
938	101 MOOBERRY ST	01-01-34-104-005	Minor Lift - .25" - 1"
940	113 FLEUR DE LIS DR	01-01-27-312-008	Minor Lift - .25" - 1"
944	323 ARNOLD AVE	01-01-33-206-037	Minor Lift - .25" - 1"
970	503 JAMES PKWY	02-02-17-313-005	Minor Lift - .25" - 1"
973	304 CYPRESS POINT	02-02-19-107-013	Minor Lift - .25" - 1"
1000	137 FLEUR DE LIS DR	01-01-27-312-029	Minor Lift - .25" - 1"
1017	131 PERSHING PL	01-01-34-103-011	Minor Lift - .25" - 1"
1019	507 JAMES PKWY	02-02-17-313-003	Minor Lift - .25" - 1"
1020	125 FLEUR DE LIS DR	01-01-27-312-032	Minor Lift - .25" - 1"
1104	120 E SHEEN AVE	01-01-34-111-007	Minor Lift - .25" - 1"
1126	207 SHADOW CREEK	02-02-19-104-004	Minor Lift - .25" - 1"
1154	150 FAWN HAVEN DR	01-01-26-310-015	Minor Lift - .25" - 1"
1183	FAIRLANE DR	01-01-14-101-010	Minor Lift - .25" - 1"
1187	114 WINDSONG DR	02-02-18-405-009	Minor Lift - .25" - 1"
1208	128 PIERSON ST	01-01-33-208-002	Minor Lift - .25" - 1"
1214	205 WHISTLING STRAIT	02-02-19-107-009	Minor Lift - .25" - 1"
1218	109 THOMAS CT	02-02-20-100-029	Minor Lift - .25" - 1"
1220	204 MADISSYN CT	01-01-24-112-006	Minor Lift - .25" - 1"
1223	110 SMILEY CT	02-02-17-305-007	Minor Lift - .25" - 1"
1224	108 WOODROW DR	01-01-34-100-008	Minor Lift - .25" - 1"
1225	211 SHADOW CREEK	02-02-19-104-006	Minor Lift - .25" - 1"
1281	2366 WASHINGTON RD	02-02-19-408-010	Minor Lift - .25" - 1"
1282	213 SHADOW CREEK	02-02-19-104-007	Minor Lift - .25" - 1"
1292	108 SHANNON HILLS DR	02-02-18-404-012	Minor Lift - .25" - 1"
1299	107 FLEUR DE LIS CT	01-01-27-312-015	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1303	104 SMILEY CT	02-02-17-302-034	Minor Lift - .25" - 1"
1307	113 WHISTLING STRAIT	02-02-19-106-004	Minor Lift - .25" - 1"
1312	130 PERSHING PL	01-01-34-104-003	Minor Lift - .25" - 1"
1330	102 MADISSYN CT	01-01-24-112-002	Minor Lift - .25" - 1"
1341	136 FLEUR DE LIS DR	01-01-27-312-024	Minor Lift - .25" - 1"
1368	132 WOODROW DR	01-01-34-100-005	Minor Lift - .25" - 1"
1370	104 FIELDCREST DR	01-01-13-102-024	Minor Lift - .25" - 1"
1372	100 MADISSYN CT	01-01-24-112-001	Minor Lift - .25" - 1"
1374	508 JAMES PKWY	02-02-17-311-022	Minor Lift - .25" - 1"
1396	FAIRLANE DR	01-01-14-101-012	Minor Lift - .25" - 1"
1419	213 BARBARA PKWY	02-02-17-304-005	Minor Lift - .25" - 1"
1425	302 BARBARA PKWY	02-02-17-305-014	Minor Lift - .25" - 1"
1430	201 SHADOW CREEK	02-02-19-104-001	Minor Lift - .25" - 1"
1448	103 WINDSONG DR	02-02-18-406-008	Minor Lift - .25" - 1"
1463	108 WINDSONG DR	02-02-18-405-012	Minor Lift - .25" - 1"
1492	210 BARBARA PKWY	02-02-17-305-033	Minor Lift - .25" - 1"
1493	105 BROOKLYN CT	01-01-24-112-022	Minor Lift - .25" - 1"
1500	224 PERSHING PL	01-01-34-102-003	Minor Lift - .25" - 1"
1511	204 PINEHURST LN	02-02-19-102-019	Minor Lift - .25" - 1"
1513	217 CYPRESS POINT	02-02-19-107-038	Minor Lift - .25" - 1"
1573	200 MADISSYN CT	01-01-24-112-004	Minor Lift - .25" - 1"
1588	203 SKYLARK LN	02-02-18-404-008	Minor Lift - .25" - 1"
1603	112 WINDSONG DR	02-02-18-405-010	Minor Lift - .25" - 1"
1621	128 FAWN HAVEN DR	01-01-26-310-014	Minor Lift - .25" - 1"
1627	300 BARBARA PKWY	02-02-17-305-013	Minor Lift - .25" - 1"
1638	109 WHISTLING STRAIT	02-02-19-106-003	Minor Lift - .25" - 1"
1639	700 GEORGIA PKWY	02-02-17-309-016	Minor Lift - .25" - 1"
1650	406 KEAYES AVE	01-01-34-109-003	Minor Lift - .25" - 1"
1683	118 WOODROW DR	01-01-34-100-007	Minor Lift - .25" - 1"
1685	203 EASTGATE DR	01-01-13-203-004	Minor Lift - .25" - 1"
1689	304 CARLOCK CT	02-02-17-305-030	Minor Lift - .25" - 1"
1693	100 SKYLARK LN	02-02-18-407-006	Minor Lift - .25" - 1"
1696	215 HARDING BLVD	01-01-34-102-014	Minor Lift - .25" - 1"
1703	104 THOMAS CT	02-02-20-100-034	Minor Lift - .25" - 1"
1725	141 FLEUR DE LIS DR	01-01-27-312-028	Minor Lift - .25" - 1"
1726	112 SHADOW CREEK	02-02-19-102-011	Minor Lift - .25" - 1"
1738	121 FAWN HAVEN DR	01-01-26-308-031	Minor Lift - .25" - 1"
1753	113 WINDSONG DR	02-02-18-406-003	Minor Lift - .25" - 1"
1783	N MAIN ST (OFF OF)	01-01-22-200-004	Minor Lift - .25" - 1"
1795	507 GEORGIA PKWY	02-02-17-311-011	Minor Lift - .25" - 1"
1803	115 OAKRIDGE CT	01-01-23-416-020	Minor Lift - .25" - 1"
1805	107 SMILEY CT	02-02-17-302-019	Minor Lift - .25" - 1"
1806	2400 WASHINGTON RD	02-02-19-314-017	Minor Lift - .25" - 1"
1813	509 JAMES PKWY	02-02-17-313-002	Minor Lift - .25" - 1"
1820	208 BARBARA PKWY	02-02-17-305-034	Minor Lift - .25" - 1"
1822	203 EASTGATE DR	01-01-13-203-004	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1849	138 FAWN HAVEN DR	01-01-26-310-021	Minor Lift - .25" - 1"
1855	100 WOODROW DR	01-01-34-100-010	Minor Lift - .25" - 1"
1865	206 CYPRESS POINT	02-02-19-106-011	Minor Lift - .25" - 1"
1911	131 FAWN HAVEN DR	01-01-26-308-042	Minor Lift - .25" - 1"
1930	215 FAIRLANE DR	01-01-14-101-018	Minor Lift - .25" - 1"
1934	110 MIROMAR LN	02-02-19-104-016	Minor Lift - .25" - 1"
1937	209 BARBARA PKWY	02-02-17-304-007	Minor Lift - .25" - 1"
2004	105 TOWER POINTE	01-01-35-425-002	Minor Lift - .25" - 1"
2011	513 S PLEASANT HILL RD	05-05-12-112-006	Minor Lift - .25" - 1"
2036	128 SPRING ST	01-01-33-416-006	Minor Lift - .25" - 1"
2049	143 ELMWOOD ST	01-01-33-421-007	Minor Lift - .25" - 1"
2072	126 CATHERINE ST	01-01-34-401-004	Minor Lift - .25" - 1"
2113	117 SUSAN CT	05-05-10-203-032	Minor Lift - .25" - 1"
2134	615 MEADOW AVE	01-01-34-402-022	Minor Lift - .25" - 1"
2152	515 S PLEASANT HILL RD	05-05-12-112-007	Minor Lift - .25" - 1"
2157	120 SPRING ST	01-01-33-416-004	Minor Lift - .25" - 1"
2210	115 CALLENDER AVE	01-01-34-402-017	Minor Lift - .25" - 1"
2290	103 ALICE AVE	05-05-09-201-061	Minor Lift - .25" - 1"
2294	111 ALICE AVE	05-05-09-201-057	Minor Lift - .25" - 1"
2386	147 ELMRIDGE CIR	05-05-09-305-016	Minor Lift - .25" - 1"
2396	117 ELMRIDGE CIR	05-05-09-305-005	Minor Lift - .25" - 1"
2406	100 ELMRIDGE CIR	05-05-09-306-001	Minor Lift - .25" - 1"
2410	116 ELMRIDGE CIR	05-05-09-306-005	Minor Lift - .25" - 1"
2413	136 ELMRIDGE CIR	05-05-09-306-008	Minor Lift - .25" - 1"
2425	326 COTTONWOOD CIR	05-05-09-307-028	Minor Lift - .25" - 1"
2431	508 COTTONWOOD CIR	05-05-09-303-049	Minor Lift - .25" - 1"
2464	801 SPRINGFIELD RD	05-05-05-204-002	Minor Lift - .25" - 1"
56	200 N MAIN ST	01-01-33-301-001	Minor Lift - .25" - 1"
100			Minor Lift - .25" - 1"
127	424 DOERING AVE	05-05-05-113-010	Minor Lift - .25" - 1"
224	346 COLE ST	01-01-33-318-032	Minor Lift - .25" - 1"
227	1 CONFERENCE CENTER	01-01-29-401-035	Minor Lift - .25" - 1"
235	235 EVERETT ST	01-01-33-313-008	Minor Lift - .25" - 1"
254	334 COLE ST	01-01-33-318-030	Minor Lift - .25" - 1"
266	501 W WASHINGTON ST	01-01-32-202-003	Minor Lift - .25" - 1"
355			Minor Lift - .25" - 1"
426			Minor Lift - .25" - 1"
445	E WASHINGTON ST	01-01-33-306-002	Minor Lift - .25" - 1"
482	129 E WASHINGTON ST	01-01-33-305-008	Minor Lift - .25" - 1"
545	301 N MAIN ST	01-01-33-100-026	Minor Lift - .25" - 1"
555	354 COLE ST	01-01-33-318-039	Minor Lift - .25" - 1"
601			Minor Lift - .25" - 1"
618	400 RICHLAND ST	01-01-32-207-002	Minor Lift - .25" - 1"
638	139 EVERETT ST	01-01-33-312-012	Minor Lift - .25" - 1"
752	111 E WASHINGTON ST	01-01-33-305-005	Minor Lift - .25" - 1"
778	E WASHINGTON ST	01-01-33-306-005	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
806	126 JOHNSON ST	01-01-32-413-018	Minor Lift - .25" - 1"
838	300 EDMUND ST	01-01-32-114-023	Minor Lift - .25" - 1"
850	140 E WASHINGTON ST	01-01-33-312-005	Minor Lift - .25" - 1"
896	SCHERTZ AVE	01-01-33-406-011	Minor Lift - .25" - 1"
916	N/A	N/A	Minor Lift - .25" - 1"
925	903 E WASHINGTON ST	01-01-33-401-018	Minor Lift - .25" - 1"
927	2201 E WASHINGTON ST	01-01-34-203-004	Minor Lift - .25" - 1"
950	2322 WASHINGTON RD	02-02-19-411-014	Minor Lift - .25" - 1"
995	125 CAROLINE ST	01-01-34-114-010	Minor Lift - .25" - 1"
1028	1109 E WASHINGTON ST	01-01-33-209-012	Minor Lift - .25" - 1"
1039	157 CAROLINE ST	01-01-34-114-017	Minor Lift - .25" - 1"
1048	N/A	N/A	Minor Lift - .25" - 1"
1052	307 LEADLEY AVE	01-01-33-212-014	Minor Lift - .25" - 1"
1055	2807 E WASHINGTON ST	01-01-26-400-008	Minor Lift - .25" - 1"
1065	825 E CAMP ST	01-01-33-401-015	Minor Lift - .25" - 1"
1073	3717 E WASHINGTON ST	01-01-25-207-022	Minor Lift - .25" - 1"
1079	2255 E WASHINGTON ST	01-01-34-203-007	Minor Lift - .25" - 1"
1111	E WASHINGTON ST	01-01-26-300-039	Minor Lift - .25" - 1"
1127	105 CAROLINE ST	01-01-34-114-007	Minor Lift - .25" - 1"
1153	E WASHINGTON ST	01-01-25-210-013	Minor Lift - .25" - 1"
1155	2201 E WASHINGTON ST	01-01-34-203-006	Minor Lift - .25" - 1"
1184	2107 E WASHINGTON ST	01-01-34-202-014	Minor Lift - .25" - 1"
1191	2801 E WASHINGTON RD	01-01-26-400-002	Minor Lift - .25" - 1"
1211	1416 E WASHINGTON ST	01-01-34-110-001	Minor Lift - .25" - 1"
1265	32 & 2236 E WASHINGTON	01-01-34-206-004	Minor Lift - .25" - 1"
1275	ILLINI DR	01-01-26-300-036	Minor Lift - .25" - 1"
1323	3905 E WASHINGTON ST	01-01-25-205-029	Minor Lift - .25" - 1"
1331	N/A	N/A	Minor Lift - .25" - 1"
1334	2535 E WASHINGTON ST	01-01-26-300-015	Minor Lift - .25" - 1"
1437	113 FAIRVIEW AVE	01-01-33-400-006	Minor Lift - .25" - 1"
1440	1303 E WASHINGTON ST	01-01-34-103-016	Minor Lift - .25" - 1"
1462	2306 E WASHINGTON ST	01-01-35-101-001	Minor Lift - .25" - 1"
1473	1101 E WASHINGTON ST	01-01-33-209-011	Minor Lift - .25" - 1"
1481	233 LEADLEY AVE	01-01-33-213-001	Minor Lift - .25" - 1"
1488	2807 E WASHINGTON ST	01-01-26-400-008	Minor Lift - .25" - 1"
1518	2045 E WASHINGTON ST	01-01-34-202-006	Minor Lift - .25" - 1"
1591	1207 E WASHINGTON ST	01-01-33-213-012	Minor Lift - .25" - 1"
1595	122 CAROLINE ST	01-01-34-113-018	Minor Lift - .25" - 1"
1615	123 CAROLINE ST	01-01-34-114-009	Minor Lift - .25" - 1"
1634	2525 E WASHINGTON ST	01-01-26-300-004	Minor Lift - .25" - 1"
1682	N/A	N/A	Minor Lift - .25" - 1"
1691	2301 E WASHINGTON ST	01-01-35-100-003	Minor Lift - .25" - 1"
1717	ROSEDALE RD	01-01-34-100-020	Minor Lift - .25" - 1"
1731	109 EDGEHILL CT	01-01-26-404-003	Minor Lift - .25" - 1"
1741	1303 E WASHINGTON ST	01-01-34-103-016	Minor Lift - .25" - 1"
1763	100 WHITE OAK LN	01-01-34-100-017	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1779	E WASHINGTON ST	01-01-25-110-032	Minor Lift - .25" - 1"
1828	1011 E WASHINGTON ST	01-01-33-206-049	Minor Lift - .25" - 1"
1851	1207 E WASHINGTON ST	01-01-33-213-012	Minor Lift - .25" - 1"
1859	3701 E WASHINGTON ST	01-01-25-207-019	Minor Lift - .25" - 1"
1860	3307 E WASHINGTON ST	01-01-25-301-011	Minor Lift - .25" - 1"
1866	ILLINI DR	01-01-26-300-036	Minor Lift - .25" - 1"
1912	3301 E WASHINGTON ST	01-01-25-301-008	Minor Lift - .25" - 1"
1931	1420 E WASHINGTON ST	01-01-34-110-018	Minor Lift - .25" - 1"
1965	N/A	N/A	Minor Lift - .25" - 1"
1967	3410 E WASHINGTON ST	01-01-25-303-003	Minor Lift - .25" - 1"
1973	3418 E WASHINGTON ST	01-01-25-303-005	Minor Lift - .25" - 1"
1977	2800 E WASHINGTON ST	01-01-26-400-012	Minor Lift - .25" - 1"
1998	109 KAREN DR	05-05-04-215-003	Minor Lift - .25" - 1"
2006	108 SUNNYBROOK DR	05-05-04-216-017	Minor Lift - .25" - 1"
2008	112 SUNNYBROOK DR	05-05-04-216-019	Minor Lift - .25" - 1"
2014	101 KAITLIN CT	05-05-04-214-009	Minor Lift - .25" - 1"
2040	116 KAREN DR	05-05-04-214-003	Minor Lift - .25" - 1"
2046	113 KAITLIN CT	05-05-04-214-012	Minor Lift - .25" - 1"
2060	121 KAITLIN CT	05-05-04-214-014	Minor Lift - .25" - 1"
2064	100 SHADOWAY DR	01-01-34-307-001	Minor Lift - .25" - 1"
2081	115 KERFOOT ST	05-05-04-200-008	Minor Lift - .25" - 1"
2085	200 SUNNYBROOK DR	05-05-04-216-022	Minor Lift - .25" - 1"
2119	VETERANS DR	01-01-34-301-021	Minor Lift - .25" - 1"
2131	104 KAREN DR	05-05-04-214-006	Minor Lift - .25" - 1"
2133	204 SUNNYBROOK DR	05-05-04-216-023	Minor Lift - .25" - 1"
2159	116 KAITLIN CT	05-05-04-214-017	Minor Lift - .25" - 1"
2164	201 KERFOOT ST	05-05-04-200-020	Minor Lift - .25" - 1"
2167	200 VETERANS DR	01-01-33-412-004	Minor Lift - .25" - 1"
2179	203 KERFOOT ST	05-05-04-200-021	Minor Lift - .25" - 1"
2180	229 SUNNYBROOK DR	05-05-04-214-027	Minor Lift - .25" - 1"
2196	445 VETERANS DR	01-01-34-301-016	Minor Lift - .25" - 1"
2213	224 SUNNYBROOK DR	05-05-04-216-035	Minor Lift - .25" - 1"
648	0 CONFERENCE CENTER	01-01-29-201-018	Minor Lift - .25" - 1"
1348	2309 WASHINGTON RD	02-02-19-417-027	Minor Lift - .25" - 1"
49	W CAMP ST (OFF OF)	01-01-29-401-040	Minor Lift - .25" - 1"
128	101 HOLIDAY ST	01-01-32-209-001	Minor Lift - .25" - 1"
219	222 JOHNSON ST	01-01-33-318-005	Minor Lift - .25" - 1"
330	101 HOLIDAY ST	01-01-32-209-001	Minor Lift - .25" - 1"
422	251 CLOCK TOWER DR	01-01-32-200-028	Minor Lift - .25" - 1"
471	741 TAYLOR ST	01-01-33-409-015	Minor Lift - .25" - 1"
524	SPINDER DR	01-01-32-206-003	Minor Lift - .25" - 1"
585	W CAMP ST	01-01-32-201-009	Minor Lift - .25" - 1"
590	201 CLOCK TOWER DR	01-01-32-200-027	Minor Lift - .25" - 1"
623	W WASHINGTON ST	01-01-32-103-004	Minor Lift - .25" - 1"
673	SPINDER DR	01-01-32-206-002	Minor Lift - .25" - 1"
676	138 CHICAGO ST	01-01-32-400-005	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
731	SPINDER DR	01-01-32-206-002	Minor Lift - .25" - 1"
745	101 HOLIDAY ST	01-01-32-209-001	Minor Lift - .25" - 1"
757	314 W WASHINGTON ST	01-01-32-205-016	Minor Lift - .25" - 1"
763	333 COLE ST	01-01-33-319-007	Minor Lift - .25" - 1"
773	480 W WASHINGTON ST	01-01-32-205-008	Minor Lift - .25" - 1"
933	205 EASTGATE DR	01-01-13-202-021	Minor Lift - .25" - 1"
941	101 SKYLARK DR	02-02-18-405-014	Minor Lift - .25" - 1"
943	100 VICTORY DR	01-01-24-310-007	Minor Lift - .25" - 1"
962	309 GREENVIEW DR	01-01-14-201-013	Minor Lift - .25" - 1"
971	304 SILVER CREEK DR	02-02-18-401-005	Minor Lift - .25" - 1"
996	143 FAWN HAVEN DR	01-01-26-308-036	Minor Lift - .25" - 1"
1013	214 BARBARA PKWY	02-02-17-305-023	Minor Lift - .25" - 1"
1023	101 GRANDEZZA DR	02-02-19-107-031	Minor Lift - .25" - 1"
1025	201 CYPRESS POINT	02-02-19-107-024	Minor Lift - .25" - 1"
1036	135 PERSHING PL	01-01-34-103-010	Minor Lift - .25" - 1"
1047	105 VICTORY DR	01-01-24-311-002	Minor Lift - .25" - 1"
1051	117 WHISTLING STRAIT	02-02-19-106-005	Minor Lift - .25" - 1"
1092	4209 E WASHINGTON ST	01-01-24-400-002	Minor Lift - .25" - 1"
1142	101 SHANNON HILLS DR	02-02-18-405-007	Minor Lift - .25" - 1"
1158	113 CYPRESS POINT	02-02-19-107-026	Minor Lift - .25" - 1"
1162	301 & 303 CYPRESS POINT	02-02-19-107-019	Minor Lift - .25" - 1"
1185	234 FAIRVIEW AVE	01-01-33-203-013	Minor Lift - .25" - 1"
1195	209 CYPRESS POINT	02-02-19-107-022	Minor Lift - .25" - 1"
1205	142 FAWN HAVEN DR	01-01-26-310-019	Minor Lift - .25" - 1"
1215	308 BROOKSTONE DR	01-01-13-102-019	Minor Lift - .25" - 1"
1231	103 SILVER CREEK DR	02-02-18-404-006	Minor Lift - .25" - 1"
1235	135 FAWN HAVEN DR	01-01-26-308-040	Minor Lift - .25" - 1"
1253	304 CYPRESS POINT	02-02-19-107-013	Minor Lift - .25" - 1"
1293	201 SILVER CREEK DR	02-02-18-404-005	Minor Lift - .25" - 1"
1314	203 WHISTLING STRAIT	02-02-19-107-010	Minor Lift - .25" - 1"
1316	100 FAWN HAVEN DR	01-01-26-310-007	Minor Lift - .25" - 1"
1320	206 SHADOW CREEK	02-02-19-102-004	Minor Lift - .25" - 1"
1357	208 PINEHURST LN	02-02-19-102-016	Minor Lift - .25" - 1"
1400	2337 WASHINGTON RD	02-02-19-416-017	Minor Lift - .25" - 1"
1412	202 MADISSYN CT	01-01-24-112-005	Minor Lift - .25" - 1"
1418	118 SHADOW CREEK	02-02-19-102-008	Minor Lift - .25" - 1"
1434	117 CYPRESS POINT	02-02-19-107-025	Minor Lift - .25" - 1"
1460	113 SHADOW CREEK	02-02-19-102-012	Minor Lift - .25" - 1"
1519	104 FAWN HAVEN DR	01-01-26-310-008	Minor Lift - .25" - 1"
1521	130 FAWN HAVEN DR	01-01-26-310-025	Minor Lift - .25" - 1"
1529	110 RUTH CT	02-02-19-107-050	Minor Lift - .25" - 1"
1531	202 SKYLARK LN	02-02-18-407-003	Minor Lift - .25" - 1"
1554	205 EASTGATE DR	01-01-13-202-021	Minor Lift - .25" - 1"
1556	321 WHISTLING STRAIT	02-02-19-107-002	Minor Lift - .25" - 1"
1566	112 FAWN HAVEN DR	01-01-26-310-010	Minor Lift - .25" - 1"
1654	207 MADISSYN CT	01-01-24-112-013	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1658	208 MADISSYN CT	01-01-24-112-008	Minor Lift - .25" - 1"
1676	102 SMILEY CT	02-02-17-302-032	Minor Lift - .25" - 1"
1702	212 SHADOW CREEK	02-02-19-102-001	Minor Lift - .25" - 1"
1720	115 FAWN HAVEN DR	01-01-26-308-030	Minor Lift - .25" - 1"
1796	113 OAKRIDGE CT	01-01-23-416-021	Minor Lift - .25" - 1"
1811	105 SHANNON HILLS DR	02-02-18-405-005	Minor Lift - .25" - 1"
1821	2464 WASHINGTON RD	02-02-19-303-035	Minor Lift - .25" - 1"
1823	505 JAMES PKWY	02-02-17-313-004	Minor Lift - .25" - 1"
1824	116 WINDSONG DR	02-02-18-405-008	Minor Lift - .25" - 1"
1825	104 PINEHURST LN	02-02-19-102-022	Minor Lift - .25" - 1"
1835	108 WHISTLING STRAIT	02-02-19-105-011	Minor Lift - .25" - 1"
1876	109 WINDSONG DR	02-02-18-406-005	Minor Lift - .25" - 1"
1902	120 FAWN HAVEN DR	01-01-26-310-012	Minor Lift - .25" - 1"
1907	103 THOMAS CT	02-02-20-100-026	Minor Lift - .25" - 1"
1922	103 SHANNON HILLS DR	02-02-18-405-006	Minor Lift - .25" - 1"
1924	2540 E WASHINGTON ST	01-01-26-300-023	Minor Lift - .25" - 1"
1946	208 CYPRESS POINT	02-02-19-106-012	Minor Lift - .25" - 1"
1954	101 SILVER CREEK DR	02-02-18-404-007	Minor Lift - .25" - 1"
1960	200 CYPRESS POINT	02-02-19-106-015	Minor Lift - .25" - 1"
1999	103 HELENE CT	05-05-02-418-028	Minor Lift - .25" - 1"
2002	427 BLOOMINGTON RD	01-01-33-421-005	Minor Lift - .25" - 1"
2096	113 SUSAN CT	05-05-10-203-033	Minor Lift - .25" - 1"
2115	102 SUSAN CT	05-05-10-203-023	Minor Lift - .25" - 1"
2155	109 TOWER POINTE	01-01-35-425-003	Minor Lift - .25" - 1"
2181	3004 BLOOMINGTON RD	05-05-02-422-002	Minor Lift - .25" - 1"
2198	106 HELENE CT	05-05-02-418-026	Minor Lift - .25" - 1"
2202	105 AMELIA DR	05-05-02-420-002	Minor Lift - .25" - 1"
2206	109 SUSAN CT	05-05-10-203-034	Minor Lift - .25" - 1"
2241	301 PEACE FIELD LN	05-05-04-407-011	Minor Lift - .25" - 1"
2249	2131 COLE ST	05-05-09-106-010	Minor Lift - .25" - 1"
2250	165 ELMRIDGE CIR	05-05-09-305-021	Minor Lift - .25" - 1"
2251	160 ELMRIDGE CIR	05-05-09-306-010	Minor Lift - .25" - 1"
2252	104 COTTONWOOD CIR	05-05-09-307-024	Minor Lift - .25" - 1"
2255	515 COTTONWOOD CIR	05-05-09-309-023	Minor Lift - .25" - 1"
2304	303 PEACE FIELD LN	05-05-04-407-012	Minor Lift - .25" - 1"
2305	305 PEACE FIELD LN	05-05-04-407-013	Minor Lift - .25" - 1"
2315	2201 COLE ST	05-05-09-106-011	Minor Lift - .25" - 1"
2318	106 STONEWOOD DR	05-05-09-108-004	Minor Lift - .25" - 1"
2319	200 STONEWOOD DR	05-05-09-108-005	Minor Lift - .25" - 1"
2320	202 STONEWOOD DR	05-05-09-108-006	Minor Lift - .25" - 1"
2321	204 STONEWOOD DR	05-05-09-108-007	Minor Lift - .25" - 1"
2323	208 STONEWOOD DR	05-05-09-108-009	Minor Lift - .25" - 1"
2324	210 STONEWOOD DR	05-05-09-108-010	Minor Lift - .25" - 1"
2325	212 STONEWOOD DR	05-05-09-108-011	Minor Lift - .25" - 1"
2330	103 PARKEDGE CT	05-05-09-108-016	Minor Lift - .25" - 1"
2339	320 STONEWOOD DR	05-05-09-108-025	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
2342	326 STONEWOOD DR	05-05-09-108-028	Minor Lift - .25" - 1"
2352	203 STONEWOOD DR	05-05-09-107-011	Minor Lift - .25" - 1"
2356	103 STONE CREEK CT	05-05-09-107-003	Minor Lift - .25" - 1"
2363	103 WOODBROOK CT	05-05-09-107-014	Minor Lift - .25" - 1"
2365	105 WOODBROOK CT	05-05-09-107-015	Minor Lift - .25" - 1"
2369	305 STONEWOOD DR	05-05-09-107-027	Minor Lift - .25" - 1"
2370	109 WOODBROOK CT	05-05-09-107-017	Minor Lift - .25" - 1"
2371	108 WOODBROOK CT	05-05-09-107-023	Minor Lift - .25" - 1"
2373	111 WOODBROOK CT	05-05-09-107-018	Minor Lift - .25" - 1"
2374	112 WOODBROOK CT	05-05-09-107-022	Minor Lift - .25" - 1"
2375	113 WOODBROOK CT	05-05-09-107-019	Minor Lift - .25" - 1"
2376	309 STONEWOOD DR	05-05-09-107-029	Minor Lift - .25" - 1"
2377	115 WOODBROOK CT	05-05-09-107-020	Minor Lift - .25" - 1"
2384	151 ELMRIDGE CIR	05-05-09-305-017	Minor Lift - .25" - 1"
2385	149 ELMRIDGE CIR	05-05-09-305-026	Minor Lift - .25" - 1"
2387	145 ELMRIDGE CIR	05-05-09-305-015	Minor Lift - .25" - 1"
2388	143 ELMRIDGE CIR	05-05-09-305-014	Minor Lift - .25" - 1"
2401	154 ELMRIDGE CIR	05-05-09-306-012	Minor Lift - .25" - 1"
2402	152 ELMRIDGE CIR	05-05-09-306-013	Minor Lift - .25" - 1"
2404	150 ELMRIDGE CIR	05-05-09-306-014	Minor Lift - .25" - 1"
2416	202 COTTONWOOD CIR	05-05-09-307-021	Minor Lift - .25" - 1"
2418	206 COTTONWOOD CIR	05-05-09-307-019	Minor Lift - .25" - 1"
2419	208 COTTONWOOD CIR	05-05-09-307-018	Minor Lift - .25" - 1"
2430	510 COTTONWOOD CIR	05-05-09-303-036	Minor Lift - .25" - 1"
2433	502 COTTONWOOD CIR	05-05-09-303-019	Minor Lift - .25" - 1"
2436	406 COTTONWOOD CIR	05-05-09-303-016	Minor Lift - .25" - 1"
2437	404 COTTONWOOD CIR	05-05-09-303-015	Minor Lift - .25" - 1"
108	119-155 CENTER ST	01-01-32-204-056	Minor Lift - .25" - 1"
363	401 W WASHINGTON ST	01-01-32-207-001	Minor Lift - .25" - 1"
576	401 W WASHINGTON ST	01-01-32-207-001	Minor Lift - .25" - 1"
615	35 SPRINGFIELD RD	01-01-33-307-008	Minor Lift - .25" - 1"
668	119-155 CENTER ST	01-01-32-204-056	Minor Lift - .25" - 1"
687	RENCE CENTER DR (PARKI	01-01-29-300-009	Minor Lift - .25" - 1"
811	WASHINGTON ST	01-01-32-204-047	Minor Lift - .25" - 1"
816	111 E WASHINGTON ST	01-01-33-305-005	Minor Lift - .25" - 1"
987	N/A	N/A	Minor Lift - .25" - 1"
1030	2306 WASHINGTON RD	02-02-19-411-015	Minor Lift - .25" - 1"
1147	3300 E WASHINGTON ST	01-01-25-304-011	Minor Lift - .25" - 1"
1319	101 DONALD CT	01-01-26-405-006	Minor Lift - .25" - 1"
1379	1825 E WASHINGTON ST	01-01-34-107-010	Minor Lift - .25" - 1"
1471	ILLINI DR	01-01-26-300-036	Minor Lift - .25" - 1"
1567	E WASHINGTON ST	01-01-25-210-013	Minor Lift - .25" - 1"
1644	3215 E WASHINGTON ST	01-01-25-301-001	Minor Lift - .25" - 1"
1836	3314 E WASHINGTON ST	01-01-25-304-017	Minor Lift - .25" - 1"
1843	3128 & 3202 E WASHINGT	01-01-26-409-005	Minor Lift - .25" - 1"
1895	E WASHINGTON ST	01-01-25-210-013	Minor Lift - .25" - 1"

SidewalkID	Location	PIN	Vertical_Lift
1952	E WASHINGTON ST	01-01-25-210-013	Minor Lift - .25" - 1"
2007	VETERANS DR	01-01-34-301-019	Minor Lift - .25" - 1"
2095	124 KAITLIN CT	05-05-04-214-015	Minor Lift - .25" - 1"
2194	112 KAITLIN CT	05-05-04-214-018	Minor Lift - .25" - 1"
2219	110 SUNNYBROOK DR	05-05-04-216-018	Minor Lift - .25" - 1"
375	1000 BASS PRO DR	01-01-28-100-006	Minor Lift - .25" - 1"
431	1201 RIVERSIDE DR	01-01-29-201-010	Minor Lift - .25" - 1"
865			Minor Lift - .25" - 1"
874	249 EDMUND ST	01-01-32-118-002	Minor Lift - .25" - 1"
1809	207 WINDSONG DR	02-02-18-402-010	Minor Lift - .25" - 1"
2350	327 STONEWOOD DR	05-05-09-107-040	Minor Lift - .25" - 1"
2353	101 STONE CREEK CT	05-05-09-107-002	Minor Lift - .25" - 1"
2354	205 STONEWOOD DR	05-05-09-107-012	Minor Lift - .25" - 1"
2360	101 WOODBROOK CT	05-05-09-107-013	Minor Lift - .25" - 1"

Obstruction

SidewalkID	Location	PIN	Sidewalk_Obstruction
2418	206 COTTONWOOD CIR	05-05-09-307-019	After meter
226	207 CLARK ST	01-01-32-109-002	Bad sidewalk heaving due to tree
1690	108 BROOKLYN CT	01-01-24-112-020	Basketball hoop
1874	101 BROOKLYN CT	01-01-24-112-024	Basketball hoop
930	104 MADISSYN CT	01-01-24-112-003	Basketball hoop
2213	224 SUNNYBROOK DR	05-05-04-216-035	Basketball hoop
1654	207 MADISSYN CT	01-01-24-112-013	Basketball hoop
277	236 E WASHINGTON ST	01-01-33-317-007	Bench
828	101 RIVER RD	01-01-29-201-015	Bench
331	200 N MAIN ST	01-01-33-301-001	Branches
956	235 ARNOLD AVE	01-01-33-206-041	Brush
455	300 CHICAGO ST	01-01-32-302-012	Brush
2165	119 CALLENDER AVE	01-01-34-402-016	Built up driveway
238	505 E WASHINGTON ST	01-01-33-324-001	Bus stop bench
1252	156 CAROLINE ST	01-01-34-113-026	Bush
2089	104 TOWER POINTE	01-01-35-425-007	Bush
2035	209 HELENE CT	05-05-02-418-037	Bush
911	1343 E WASHINGTON ST	01-01-34-104-010	Bush
2362	104 STONE CREEK CT	05-05-09-107-008	Bush
2012	112 KAREN DR	05-05-04-214-004	Bush
211	408 MONSON ST	01-01-32-117-011	Bush
67	140 FRANKLIN ST	01-01-32-118-021	Bush
303	229 SPRINGFIELD RD	01-01-33-326-010	Bush is overgrown and covering sidewalk
1064	103 KEAYES AVE	01-01-33-406-005	Bush overgrown
493	128 FRANKLIN ST	01-01-32-118-019	Bush, tree, mud, gravel
1112	132 FLEUR DE LIS DR	01-01-27-312-023	Bushes
1493	105 BROOKLYN CT	01-01-24-112-022	Bushes
2023	127 CALLENDER AVE	01-01-34-402-018	Bushes overgrowth
938	101 MOOBERRY ST	01-01-34-104-005	Bushes overgrowth- recently trimmed back but will be in the way soon enough - fire hydrant also
2249	2131 COLE ST	05-05-09-106-010	Bushes starting to grow over sidewalk
42	354 CENTER ST	01-01-32-401-029	Bushes, asphalt patch
138	231 ELM ST	01-01-32-114-007	Cars, tables
473	409 RICHLAND ST	01-01-32-204-015	Chairs

SidewalkID	Location	PIN	Sidewalk_Obstruction
1677	1343 E WASHINGTON ST	01-01-34-104-010	Cintylink bench
1646	WASHINGTON RD	02-02-19-411-013	City link bench
1029	2376 WASHINGTON RD	02-02-19-407-011	City link bench
1564	100 ALBERT ST	02-02-19-312-020	City link bench
1555	4101 E WASHINGTON ST	01-01-25-209-006	City link bench
1490	2459 WASHINGTON RD	02-02-19-316-006	City link bench
253			City link bench
1821	2464 WASHINGTON RD	02-02-19-303-035	City link bench
743	213 E WASHINGTON ST	01-01-33-311-014	City link bench and sign
375	1000 BASS PRO DR	01-01-28-100-006	Communications lid
1439	130 PIERSON ST	01-01-33-208-001	Concrete step
833	249 EDMUND ST	01-01-32-118-002	Concrete thing, trees, mud
874	249 EDMUND ST	01-01-32-118-002	Concrete thing, trees, mud
836	140 N MAIN ST	01-01-33-302-001	Covered in gravel
647	200 S MAIN ST	01-01-32-405-017	Crumbling sidewalk into roadway
561	141 EVERETT ST	01-01-33-312-013	Curb stop
2470	205 ALICE AVE	05-05-04-400-022	Curb stop
216	143 EVERETT ST	01-01-33-312-015	Curb stop
2041	108 KAREN DR	05-05-04-214-005	Curb stop
817	150 JOHNSON ST	01-01-32-413-012	Curb stop
448	141 STEWART ST	01-01-33-317-028	Curb stop
260	307 S MAIN ST	01-01-32-413-002	Dirt
658	100 MONSON ST	01-01-32-105-005	Dirt and gravel covering a good portion, excessive sloping in an area
830	334 CENTER ST	01-01-32-401-024	Dirt, grass growth, pine needles
619	120 CENTER ST	01-01-32-202-007	Dirt, gravel. Bit of sinking
491	310 MAPLE ST	01-01-32-104-013	Down wire, extreme sidewalk Heaving
2100	108 W BLUFF ST	01-01-34-404-014	Driveway gravel
1428	2250 WASHINGTON ST	01-01-34-206-006	Electric box is proud of sidewalk
524	SPINDER DR	01-01-32-206-003	Electric grate/door
1280	2445 WASHINGTON RD	02-02-19-316-012	Electric manhole is flush
1599	E WASHINGTON ST	01-01-25-210-006	Electric vault proud of sidewalk surface
58	215 CLARK ST	01-01-32-109-019	Extreme sidewalk heaving due to trees
212	333 CHICAGO ST	01-01-32-306-005	Eye level thorn bush

SidewalkID	Location	PIN	Sidewalk_Obstruction
1239	E WASHINGTON ST	01-01-25-210-013	Fiber optics vault lid is proud of sidewalk surface
1349	140 WOODROW DR	01-01-34-100-003	Fire hydrant
61	364 COLE ST	01-01-33-318-035	Fire hydrant
278	127 STEWART ST	01-01-33-317-018	Fire hydrant
1197	114 ARNOLD AVE	01-01-33-213-006	Fire hydrant
1982	113 SMILEY CT	02-02-17-305-010	Fire Hydrant
1851	1207 E WASHINGTON ST	01-01-33-213-012	Fire hydrant
1856	203 SILVER CREEK DR	02-02-18-404-004	Fire hydrant in sidewalk
1132	300 ARNOLD AVE	01-01-33-212-008	Fire hydrant, water valve sticking out 3"
96	FREVENT AVE	05-05-12-110-006	Flush utility lid
664	266 E WASHINGTON ST	01-01-33-317-010	Full of rocks
2339	320 STONEWOOD DR	05-05-09-108-025	Glob of dry concrete
766	308 CENTER ST	01-01-32-401-019	Grass
417	143 FRANKLIN ST	01-01-32-203-016	Grass
400	316 CENTER ST	01-01-32-401-021	Grass
453	336 CENTER ST	01-01-32-401-025	Grass growth
2186	125 KENWOOD AVE	01-01-34-401-013	Grass overgrown
627	304 CENTER ST	01-01-32-401-018	Grass overgrowth
542	312 CENTER ST	01-01-32-401-020	Grass overgrowth
563	304 1/2 CENTER ST	01-01-32-401-017	Grass patches, areas of missing sidewalk
851	340 CENTER ST	01-01-32-401-026	Grass, sunken area
432	222 FRANKLIN ST	01-01-32-307-021	Grass/overgrown
391			Grate with vertical lift
753			Grate with vertical lift
355			Grate with vertical lift
426			Grate with vertical lift
192	430 MONSON ST	01-01-32-117-020	Gravel
2018	127 KENWOOD AVE	01-01-34-401-012	Gravel driveway
291	412 MONSON ST	01-01-32-117-012	Gravel, pond, mud
669	418 MONSON ST	01-01-32-117-018	Gravel, pounding of water
29	217 FRANKLIN ST	01-01-32-401-038	Gravel/ overgrowth
177	147 FRANKLIN ST	01-01-32-400-001	Handicap ramp, trash can
106	113-115 E WASHINGTON ST	01-01-33-305-006	Hydrant

SidewalkID	Location	PIN	Sidewalk_Obstruction
227	1 CONFERENCE CENTER	01-01-29-401-035	Hydrant/light poles/trees
693	111 WASHINGTON ST	01-01-32-405-020	Landscape
132	127 SPRINGFIELD RD	01-01-33-325-011	Landscaping rocks falling onto sidewalk
1921	110 PIERSON ST	01-01-33-208-008	Landscaping rocks stick out a bit, easy to get caught on
1734	112 PIERSON ST	01-01-33-208-007	Landscaping rocks stick out over sidewalk. Falling onto sidewalk. Basketball hoop
567	107-179 N MAIN ST	01-01-33-102-010	Light pole
736	250- 270 S MAIN ST	01-01-32-406-021	Light pole
770	RIVER RD	01-01-29-201-021	Light pole
482	129 E WASHINGTON ST	01-01-33-305-008	Light pole
363	401 W WASHINGTON ST	01-01-32-207-001	Light pole
220	201-205 RIVER RD	01-01-29-201-023	Light pole and street sign
218	300 E WASHINGTON ST	01-01-33-317-011	Light pole, multiple utility poles, not wheelchair accessible at all
27	401 W WASHINGTON ST	01-01-32-207-001	Light poles
460	360 W WASHINGTON ST	01-01-32-205-010	Light poles and hydrant
1234	6 RONALD CT	02-02-17-311-019	Mailbox
1631	8 RONALD CT	02-02-17-311-020	Mailbox
1910	7 RONALD CT	02-02-17-311-016	Mailbox
1520	5 RONALD CT	02-02-17-311-015	Mailbox
810	126 FLOYD ST	01-01-33-323-001	Major overgrowth
784			Man hole with vertical lift and some overgrowth
1581	FAIRLANE DR	01-01-14-101-006	Manhole
1188	312 LEADLEY AVE	01-01-34-101-006	Manhole
1582	300 WHISTLING STRAIT	02-02-19-104-020	Manhole
1915	235 PERSHING PL	01-01-34-101-010	Manhole
2422	304 COTTONWOOD CIR	05-05-09-307-033	Manhole
771	200 S MAIN ST	01-01-32-405-017	Manhole
321	155 E WASHINGTON ST	01-01-33-306-003	Manhole
352	201 S MAIN ST	01-01-32-414-008	Manhole
2403	164 ELMRIDGE CIR	05-05-09-306-009	Manhole
1382	FAIRLANE DR	01-01-14-101-005	Manhole
1565	138 E SHEEN AVE	01-01-34-111-017	Manhole
1758	106 BROOKLYN CT	01-01-24-112-019	Manhole
2406	100 ELMRIDGE CIR	05-05-09-306-001	Manhole

SidewalkID	Location	PIN	Sidewalk_Obstruction
2410	116 ELMRIDGE CIR	05-05-09-306-005	Manhole
2413	136 ELMRIDGE CIR	05-05-09-306-008	Manhole
1828	1011 E WASHINGTON ST	01-01-33-206-049	Manhole
2252	104 COTTONWOOD CIR	05-05-09-307-024	Manhole
2070	131 SPRING ST	01-01-33-417-017	Manhole, foliage
539	129 E WASHINGTON ST	01-01-33-305-008	Metal sheet
1224	108 WOODROW DR	01-01-34-100-008	More of a hazard - retaining wall could give at any time!
146	105 MONSON ST	01-01-32-106-002	Mud
329	131 E WASHINGTON ST	01-01-33-305-009	Multiple signs
1481	233 LEADLEY AVE	01-01-33-213-001	Multiple signs along sidewalk
1287	221 KEAYES AVE	01-01-33-407-019	Old sign post has broken off
51	402 MONSON ST	01-01-32-117-010	Overgrown
252	239 CHICAGO ST	01-01-32-307-006	Overgrown
677	207 FRANKLIN ST	01-01-32-401-036	Overgrown weeds
406	145 CHICAGO ST	01-01-32-401-001	Overgrown weeds
168	143 CHICAGO ST	01-01-32-401-030	Overgrown weeds
421	247 CHICAGO ST	01-01-32-307-004	Overgrown, mud
304	243 CHICAGO ST	01-01-32-307-005	Overgrown, mud
761			Overgrowth
97			Overgrowth along edges
198			Overgrowth along the edges
875			Overgrowth along the edges
326	711 S MAIN ST	01-01-32-409-002	Overgrowth/gravel
892	ROSEDALE RD	01-01-34-100-015	Parking Blocks
1942	121 FLEUR DE LIS DR	01-01-27-312-033	Plant
206	120 CHICAGO ST	01-01-32-400-007	Plant/brush growth on property side
248	218 ANNA ST	01-01-33-303-002	Pole
368	113-115 E WASHINGTON ST	01-01-33-305-006	Pole
1868	124 PUTNAM ST	01-01-34-110-013	Pole
241	210 ANNA ST	01-01-33-303-004	Pole
760	215 EVERETT ST	01-01-33-313-013	Pole
591	W CAMP ST	01-01-32-201-009	Pole
161	200 ANNA ST	01-01-33-303-006	Pole

SidewalkID	Location	PIN	Sidewalk_Obstruction
81	140 E WASHINGTON ST	01-01-33-312-002	Pole
735	123 EVERETT ST	01-01-33-312-011	Poles
414	444 E WASHINGTON ST	01-01-33-325-017	Poles
380	444 E WASHINGTON ST	01-01-33-325-017	Poles
427	105 N MAIN ST	01-01-33-102-014	Railroad lights
46			Railroad lights and railroad
1473	1101 E WASHINGTON ST	01-01-33-209-011	Raised utility vault
344	328 CENTER ST	01-01-32-401-023	Rocks over driveway - most seems to be sunken below grade
829	352 CENTER ST	01-01-32-401-028	Rocks over driveway, missing areas, cracks throughout
550	122 FRANKLIN ST	01-01-32-118-018	Root
629	122 FRANKLIN ST	01-01-32-118-018	Roots
626	417 MONSON ST	01-01-32-118-011	Roots, trees, mud
1452	209 KEAYES AVE	01-01-33-407-013	Sanitary lid sits proud of sidewalk
714	617 S MAIN ST	01-01-32-409-014	Sanitary manhole
799			Sidewalk below curb - tripping hazard
84			Sidewalk is in pretty good shape, just sits 2 inches below the curb
1602	141 PUTNAM ST	01-01-34-113-011	Sign
565	338 COLE ST	01-01-33-318-031	Sign
337	109 E WASHINGTON ST	01-01-33-305-004	Sign post
801	142 FRANKLIN ST	01-01-32-118-022	Some mud
505	1000 BASS PRO DR	01-01-29-202-005	Standing water/mud
422	251 CLOCK TOWER DR	01-01-32-200-028	Stop light
242	214 COLE ST	01-01-33-315-030	Stop sign
233	117-119 E WASHINGTON ST	01-01-33-305-007	Store items and utility pole
1347	2200 E WASHINGTON ST	01-01-34-205-003	Storm inlets are raised from sidewalk
1233	2480 WASHINGTON RD	02-02-19-300-038	Street sign
488	434 MONSON ST	01-01-32-117-021	Sump discharge
712	310 CHICAGO ST	01-01-32-302-009	Sunken area w/ Asphalt overlay
600	348 CENTER ST	01-01-32-401-027	Sunken area, rocks over driveway
1213	1100 E WASHINGTON ST	01-01-33-216-003	Traffic signal box
1760	N/A	NA	Traffic signal box
1415	150-2516 E WASHINGTON ST	01-01-35-105-006	Traffic signal vault is proud of sidewalk, city link bench
1878	927 E WASHINGTON ST	01-01-33-402-014	Traffic signals box

SidewalkID	Location	PIN	Sidewalk_Obstruction
968	200 PERSHING PL	01-01-34-102-009	Tree
1532	102 MIROMAR LN	02-02-19-104-012	Tree
174	215 GRANDVIEW AVE	01-01-32-419-003	Tree
1491	104 MIROMAR LN	02-02-19-104-013	Tree
283	218 CLARK ST	01-01-32-108-005	Tree
646	139 STEWART ST	01-01-33-317-020	Tree
1604	142 CAROLINE ST	01-01-34-113-022	Tree
782	1000 BASS PRO DR	01-01-28-100-007	Tree over path
596	236 CLARK ST	01-01-32-108-009	Trees
176	249 EDMUND ST	01-01-32-118-002	Trees, roots, mud
746	134 FRANKLIN ST	01-01-32-118-020	Truck, gravel
2295	113 ALICE AVE	05-05-09-201-056	Underground cable box
445	E WASHINGTON ST	01-01-33-306-002	Utility access boxes that are slightly raised
2167	200 VETERANS DR	01-01-33-412-004	Utility box is not flush
1695	N/A	N/A	Utility box is proud of sidewalk surface
1558	E WASHINGTON ST	01-01-25-210-013	Utility box lid is proud of sidewalk surface
1348	2309 WASHINGTON RD	02-02-19-417-027	Utility box protudes
804	100 CHICAGO ST	01-01-32-400-011	Utility cover in sidewalk
2153	152 SPRING ST	01-01-33-416-012	Utility lid
950	2322 WASHINGTON RD	02-02-19-411-014	Utility lid is proud of sidewalk a scosh
1887	1343 E WASHINGTON ST	01-01-34-104-010	Utility lid on flush
240	201 S MAIN ST	01-01-32-414-008	Utility pole
476	127 ALMIRON ST	01-01-33-313-010	Utility pole
268	405 RICHLAND ST	01-01-32-204-016	Utility pole
755	244 EDMUND ST	01-01-32-111-011	Utility pole
434	RICHLAND ST	01-01-32-103-002	Utility pole
378	145 JOHNSON ST	01-01-33-314-013	Utility pole
91	312 EDMUND ST	01-01-32-114-021	Utility pole
205	252 E WASHINGTON ST	01-01-33-317-009	Utility pole
617	222 ANNA ST	01-01-33-303-001	Utility pole
689	300 SPRINGFIELD RD	01-01-33-320-010	Utility pole
108	119-155 CENTER ST	01-01-32-204-056	Utility pole
415	105 E WASHINGTON ST	01-01-33-305-002	Utility pole and wires

SidewalkID	Location	PIN	Sidewalk_Obstruction
1727	139 FAWN HAVEN DR	01-01-26-308-038	Vegetation
1806	2400 WASHINGTON RD	02-02-19-314-017	Vertical lift at manhole
1319	101 DONALD CT	01-01-26-405-006	Vertical lift from fiber vault
342			Water and gas valves
2091	137 SPRING ST	01-01-33-417-011	Water meter
1673	301 ARNOLD AVE	01-01-33-206-040	Water meter
1730	316 ARNOLD AVE	01-01-33-212-005	Water meter
1373	104 FLEUR DE LIS CT	01-01-27-312-014	Water meter
1116	301 CARLOCK CT	02-02-17-305-026	Water meter
1981	113 SMILEY CT	02-02-17-305-010	Water Meter
969	1500 FONDULAC DR	01-01-27-312-001	Water meter
1551	116 FAWN HAVEN DR	01-01-26-310-011	Water meter
1699	105 FLEUR DE LIS CT	01-01-27-312-016	Water meter
1712	128 PUTNAM ST	01-01-34-110-014	Water meter
1062	FAIRLANE DR	01-01-14-101-007	Water meter
2121	201 HELENE CT	05-05-02-418-039	Water meter
1437	113 FAIRVIEW AVE	01-01-33-400-006	Water meter
2040	116 KAREN DR	05-05-04-214-003	Water meter
2255	515 COTTONWOOD CIR	05-05-09-309-023	Water meter
13	117-119 E WASHINGTON ST	01-01-33-305-007	Water meter depression
477	128 CENTER ST	01-01-32-202-010	Water meter flush with pavement
1316	100 FAWN HAVEN DR	01-01-26-310-007	Water meter is proud of sidewalk surface
247	333 EDMUND ST	01-01-32-117-006	Water shut off and planter bed
2082	110 SUSAN CT	05-05-10-203-018	Water valve
2290	103 ALICE AVE	05-05-09-201-061	Water valve
2087	120 SUNNYBROOK DR	05-05-04-216-021	Water valve above grade
1888	217 HARDING BLVD	01-01-34-102-013	Water valve is not flush with sidewalk
1881	207 E SHEEN AVE	01-01-34-109-018	Water valve not flush with sidewalk surface
1703	104 THOMAS CT	02-02-20-100-034	Water valve not flush with sidewalk. Proud about 2"
2389	139 ELMRIDGE CIR	05-05-09-305-025	Water valve protruding above sidewalk
1072	217 KEAYES AVE	01-01-33-407-017	Water valve sits well above grade
2399	129 ELMRIDGE CIR	05-05-09-305-008	Water valve that protrudes above sidewalk, tripping hazard
1308	2467 WASHINGTON RD	02-02-19-316-004	Water. Alive and electric box

SidewalkID	Location	PIN	Sidewalk_Obstruction
354	109 E WASHINGTON ST	01-01-33-305-004	Weird storm drain extension?
821	205 N MAIN ST	01-01-33-102-011	Yellow pole

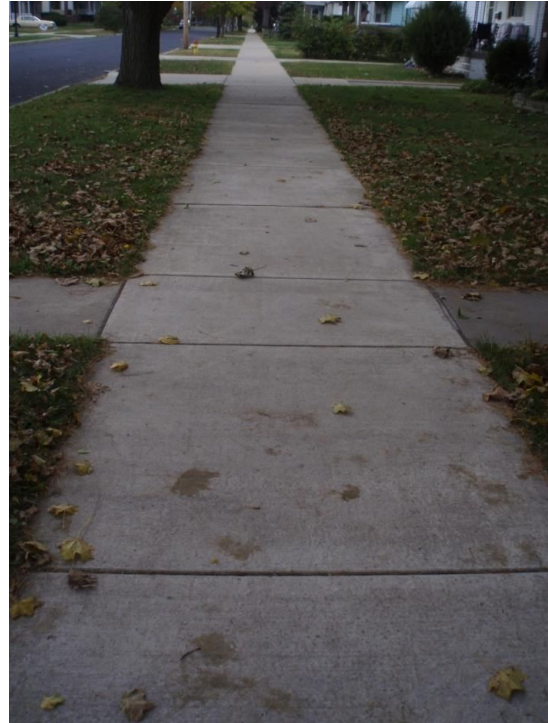
## Rating System

## **RATING SYSTEM**

This rating system is a systematic and picture-based evaluation. The actual rating is up to the interpretation of the data collector, but this manual is provided to give a basis for ratings and a common reference for any persons evaluating sidewalk. When rating the sidewalks, immediate dangers and overall quality of the sidewalk are of primary importance. In the case of vertical displacement, where a part of the sidewalk is raised thus creating a lip or tripping hazard, a higher percentage shall be given. Width of the sidewalk also matters. If the segment is damaged, a narrow sidewalk makes it more difficult to avoid the damaged section and if the sidewalk is not wide enough, it will not meet the basic ADA standards. Another important factor is the users of the sidewalk. A sidewalk near a school or place of worship will have many more pedestrians and therefore is a higher priority than a sidewalk in a less used area. However, if it is obvious the sidewalk appears to be rapidly deteriorating a worse rating should be issued as the sidewalk in question will need attention sooner than later.

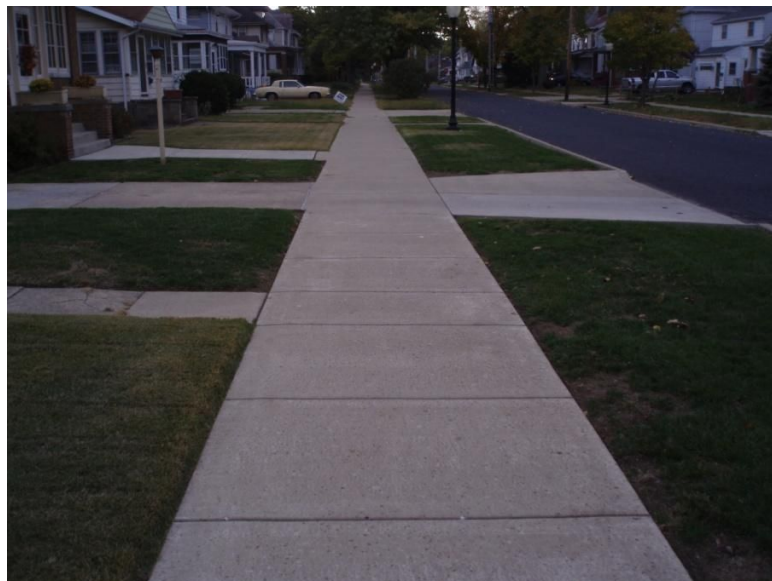
## **0% Rating**

A 0% rating is a brand new sidewalk or one still in near perfect condition. No cracks or spalling are visible and no obvious wear is present. The sidewalk should have a safe grade and cross slope with no tripping hazards. A safe and usable width of at least 4 feet is also required.. Figures 1 and 2 are examples of safe and acceptable 0% ratings. Figure 1 is slightly dirty but is still structurally intact. Figure 2 is an example of a brand new sidewalk in perfect condition.



**Figure 1. A safe, sufficient sidewalk of 0%.**

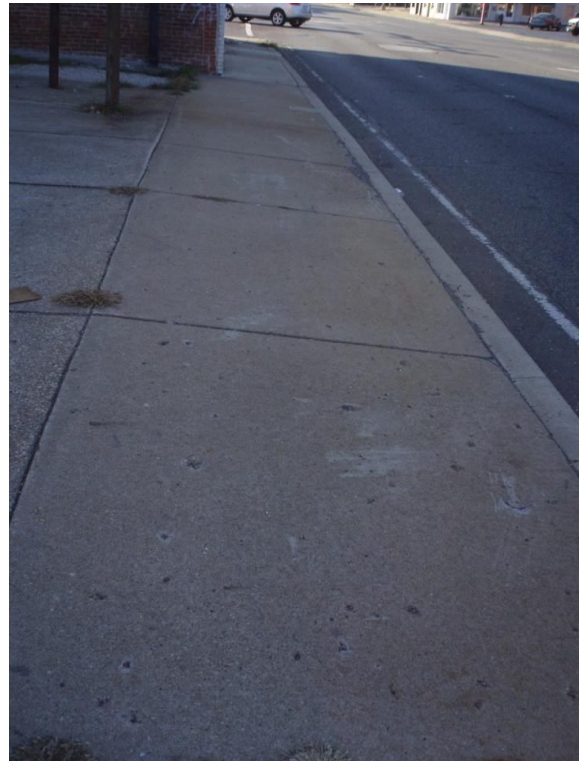
**Figure 2. Brand New Sidewalk.**



## **10% Rating**

A 10% rating has very little wear on the sidewalk. Only about 10% needs replaced, so if ten squares exist, only one of the tiles would need repair or replacement. If the whole sidewalk has a minor problem, such as spalling, a 10% rating is suitable depending on the severity of the spalling. Figures 3, 4, and 5 all show some minor spalling and cracking while all maintain structural and functional integrity.

**Figure 3. Minor Spalling.**



**Figure 4. Small amounts of spalling.**



**Figure 5. Cracking and spalling**

## **20% Rating**

A 20% rating is when either 20% of the sidewalk tiles need replaced or there is slight damage throughout the sidewalk that does not warrant total replacement. This is done because in a few years it may deteriorate to the point that it requires replacement. In Figure 6 to the right, an unsafe displacement exists which creates a tripping hazard.

Therefore, that tile needs to be replaced but as is visible, the rest of the sidewalk is in great condition and is completely safe. Figure 7 shows another mostly good sidewalk but a few tiles have footprints in them. The footprints were attempted to be patched but still present a rough walking surface.



**Figure 6. Unsafe Lip.**



**Figure 7. Footprints Poorly Patched.**

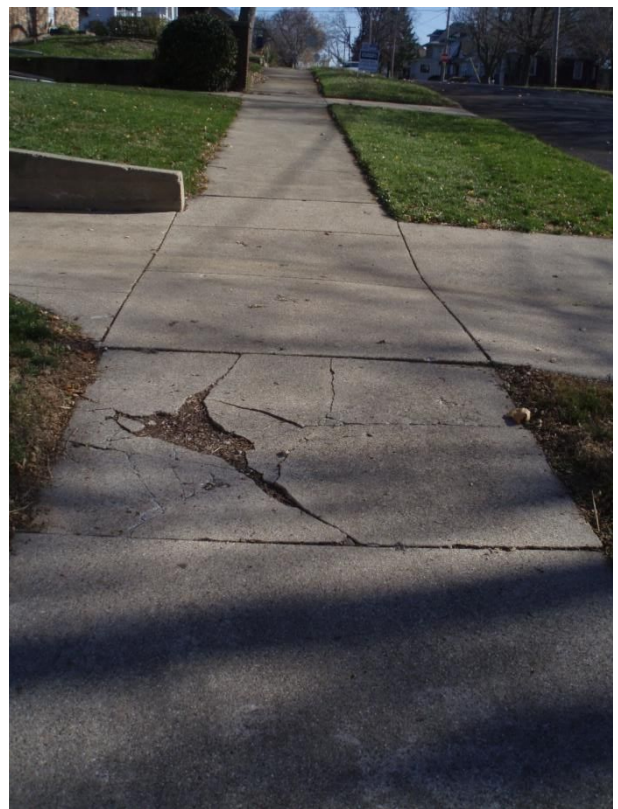
### **30% Rating**

Sidewalk would be given this rating when 30% of sidewalk tiles require replacement or patching. However, as seen in Figure 8 below, when spalling and minor cracking overlap give a 30% rating even though the sidewalk is still structurally stable and safe. When these two issues overlap even in a very small amount it decreases the time it will take to degrade to a greater magnitude.

Figure 9 presents a different story. One tile is completely destroyed and some vertical lift exists. This creates a tripping hazard and would make it difficult for wheelchair access. However, this is the only damaged part of the parcel's sidewalk so only a 30% rating is warranted.



**Figure 8. Minor Spalling and Cracking of a Sidewalk.**



**Figure 9. 30% of Segment Destroyed.**

## **40% Rating**

When two-fifths of the sidewalk is bad, a 40% replacement rating is given. In Figure 10 below, the sidewalk is structurally sound as very few cracks are present; however, initials and footprints were imprinted when the sidewalk was originally poured. These imperfections make the sidewalk uneven, rough, and therefore not safe. Without the footprints, the sidewalk would be rated at 10% or 20%. The middle range of values is where the most interpretation exists. Oddities like footprints or one extremely uneven and dangerous sidewalk tile happen and need to be accounted for in the rating evaluation. Figure 11 presents a vertical lift tripping hazard and cracking along with a narrowing of the sidewalk. All of these contribute to a small, but unacceptable segment of sidewalk.



**Figure 10. Footprints in Sidewalk**



**Figure 11. Vertical Lift in SW**

## **50% Rating**

Approximately half of the sidewalk requires some form of significant repair or replacement in this case. In Figure 12, cracks and wear are visible throughout parts of the sidewalk. No immediate safety hazards present themselves but much of the sidewalk requires work to be suitable. Figure 13 shows much more cracking and damage. This is part of a larger parcel of sidewalk section. Only the damaged half was photographed, but this section clearly requires total replacement as it is completely destroyed.



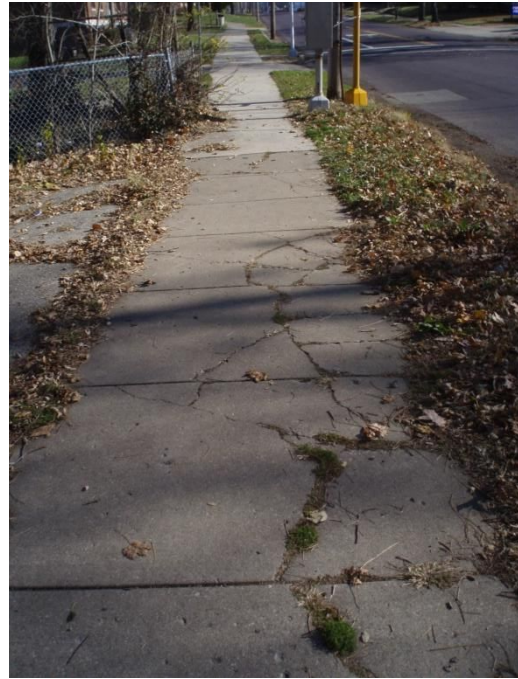
**Figure 12. Cracks and Footprints in SW Segment.**



**Figure 13. Half of Segment Destroyed (Other Half is Good).**

## **60% Rating**

When a clear majority of the sidewalk needs replaced it may mandate a 60% rating. Figure 14 shows a section of sidewalk with lots of cracking but little to no vertical lift. It is also wide enough to avoid most of the cracking, which preserves part of the safety of the sidewalk section. Figure 15 on the other hand shows a sidewalk with some vertical lift between sidewalk sections and minor cracking and spalling. A 60% rating would be justified based on the safety hazard of the lip.



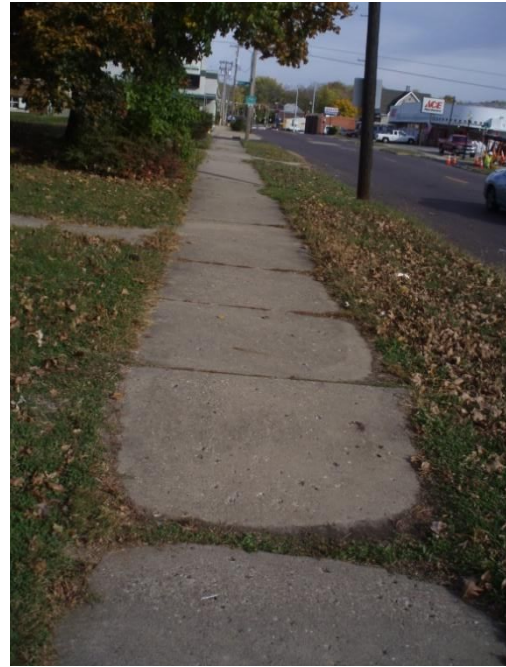
**Figure 14. Major Cracking with No Vertical Lift.**



**Figure 15. Vertical Lift with Spalling and Cracking.**

## **70% Rating**

Figure 16 depicts a sidewalk that has settled an extreme amount and has an extremely unsafe slope. This slope encouraged the 70% rating because it is much higher priority than what it would have normally been rated much lower (30-40%). Figure 17 is sloped towards the house, which is not ideal for drainage and is cracked in several places throughout the sidewalk. Figure 18 indicates a sidewalk that has cracked and broken all over approximately 70% of the sidewalk in the lot.



**Figure 16. Unsafe Dip in SW.**



**Figure 17. Sloped and Broken SW.**



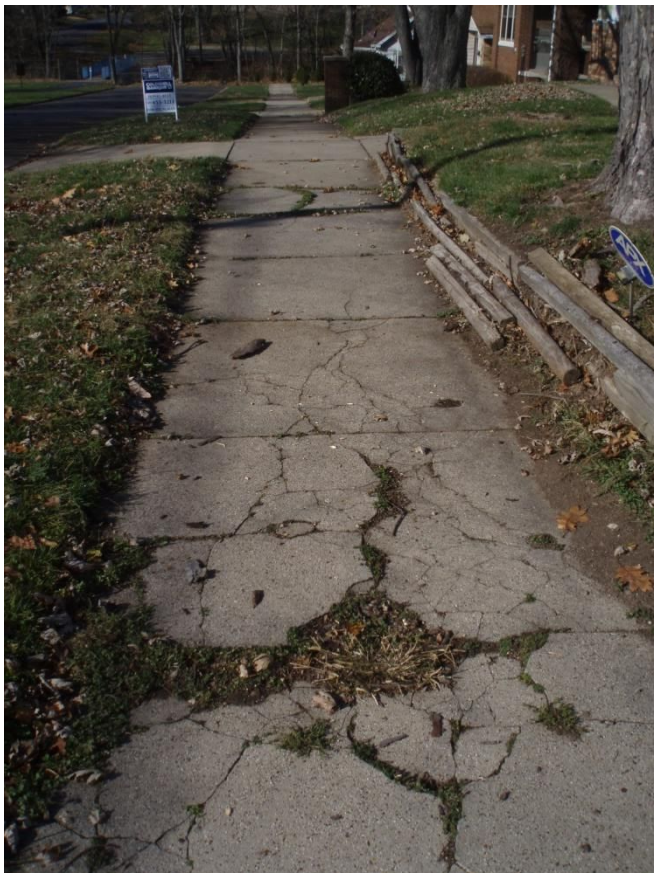
**Figure 18. Completely Broken SW.**

## **80% Rating**

The sidewalk section shown in Figure 19 is a very small section of sidewalk. When the section is this small, an 80% rating seems high, but it is a section-by-section or parcel-by-parcel basis. Figure 20 is a good representation of a larger scale sidewalk section that is almost completely destroyed. Ten sidewalk tiles exist and eight of them are cracked beyond repair and need to be replaced.



**Figure 19. Small Section of Broken SW.**



**Figure 20. 80% of SW Completely Destroyed.**

## **90% Rating**

When nearly the entire sidewalk is cracked or spalled or falling apart, ninety percent is given. Severity and quantity of cracks are illustrated in Figure 21. The sidewalk is settling and becoming uneven. Figure 22 indicates a very long crack extending across a majority of the sidewalk tiles. While this type of crack is not always an immediate hazard, the sidewalk still needs to be replaced, as it will quickly deteriorate from that point.



**Figure 21. Nearly all of SW Broken/Cracked**



**Figure 22. Crack Through Almost Whole SW.**

## **100% Rating**

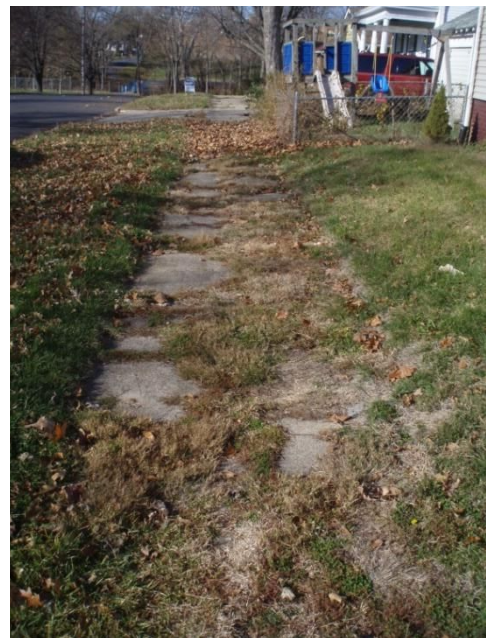
A 100 % rating is only given in the case the entire sidewalk is in poor condition or is entirely unsafe. These sidewalks are in immediate need of repair as they are a hazard for anyone to use regardless of disability. Figure 23 indicates a sidewalk that is cracked up and down the length of the property and has grass protruding through the cracks covering a majority of the tile and Figure 25 shows a completely overgrown sidewalk section. Figure 24 indicates a major crack with vertical lift throughout the entire sidewalk.



**Figure 23. Completely Destroyed SW.**



**Figure 24. Major Crack with Vertical Lift Thru Entire SW.**



**Figure 25. Cracked and Overgrown SW.**

## Examples of Deteriorated Sidewalks



















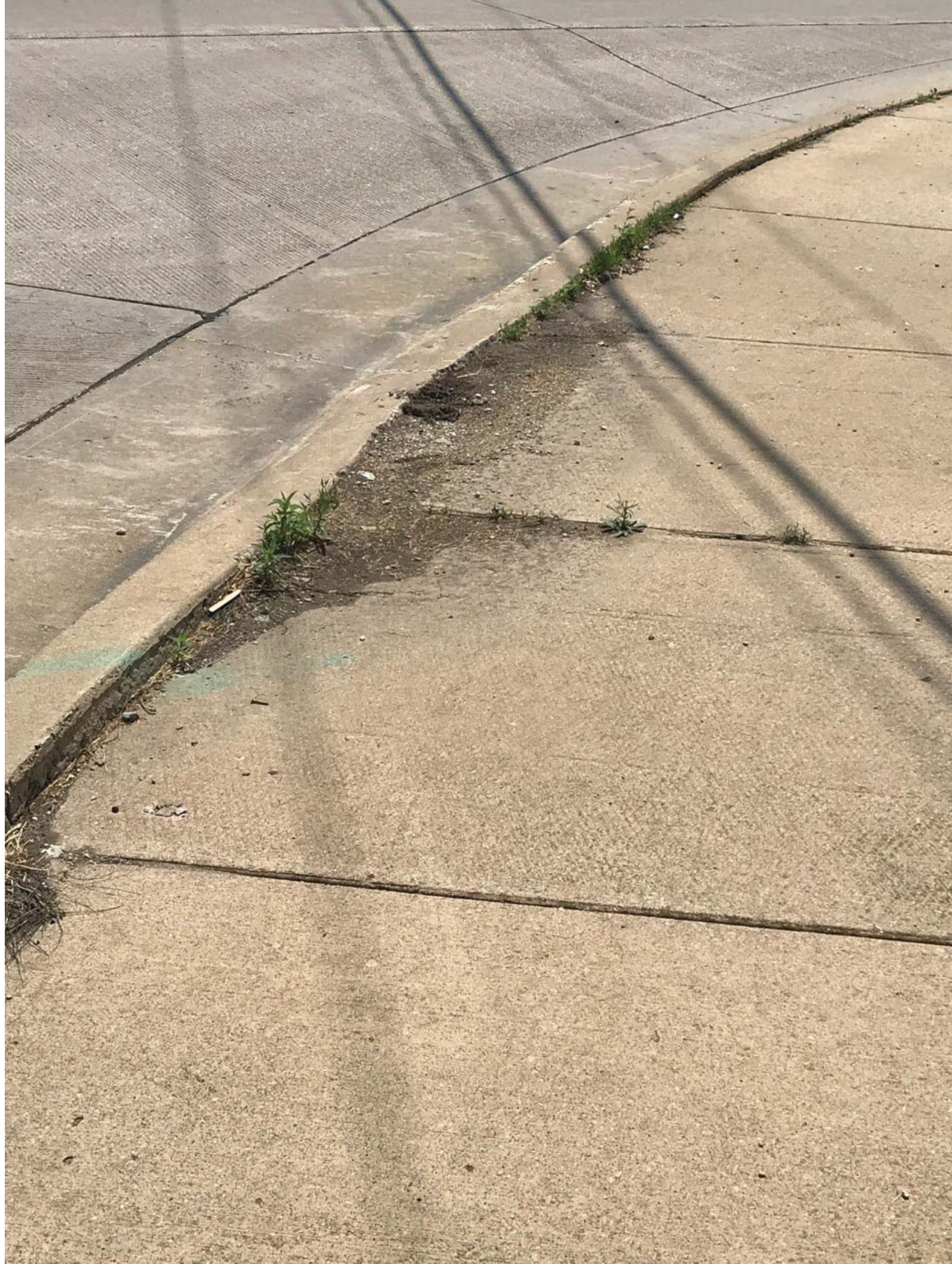
## Examples of Non-Compliant ADA Ramps



































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JCT 116

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EAST PIEDMONT

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## **Exhibit B**

# PROWAG - Alterations

- Meet New Construction Guidelines to Maximum Extent Feasible
- Document



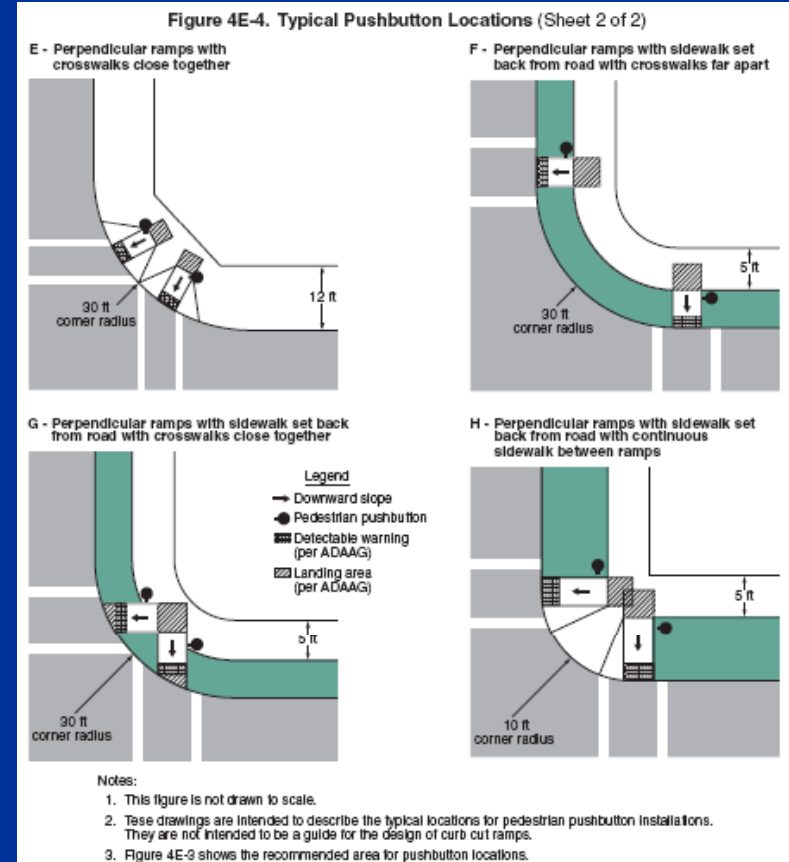
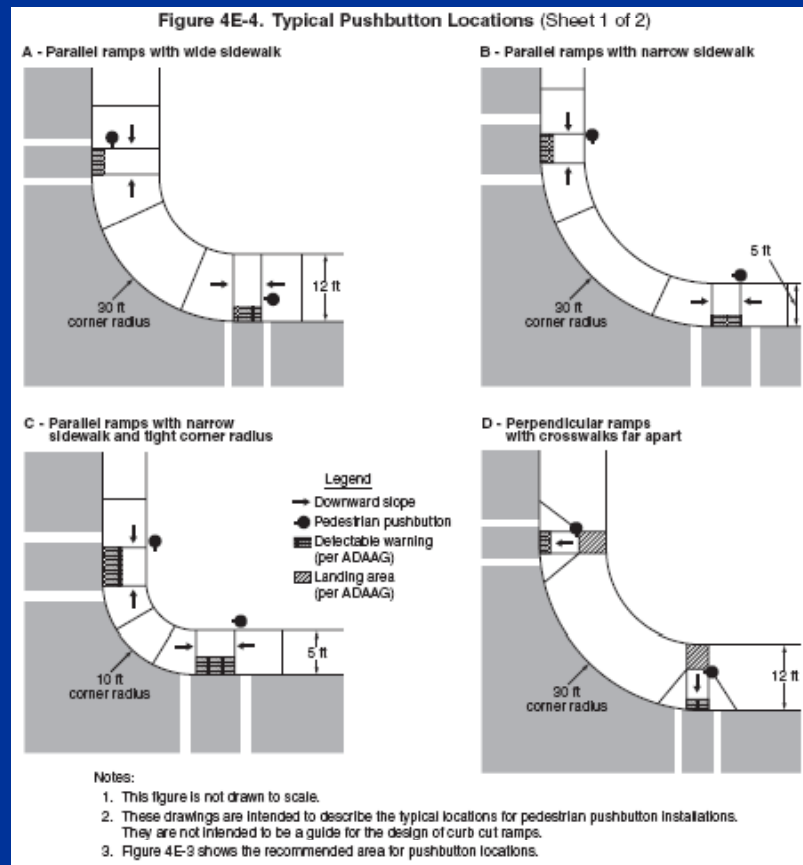
RESURFACING



RECONSTRUCTION

# PROWAG – Other References

## Usable Information (from MUTCD)



# Ped Access Route - Width

- 4 ft Min Continuous/Unobstructed Clear Width
- 5 ft Passing Space @ 200 ft



# PAR - Width



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# PAR – Cross Slope

- Limit PAR cross slope to a maximum of 2.0%, measured perpendicular to the PAR centerline.

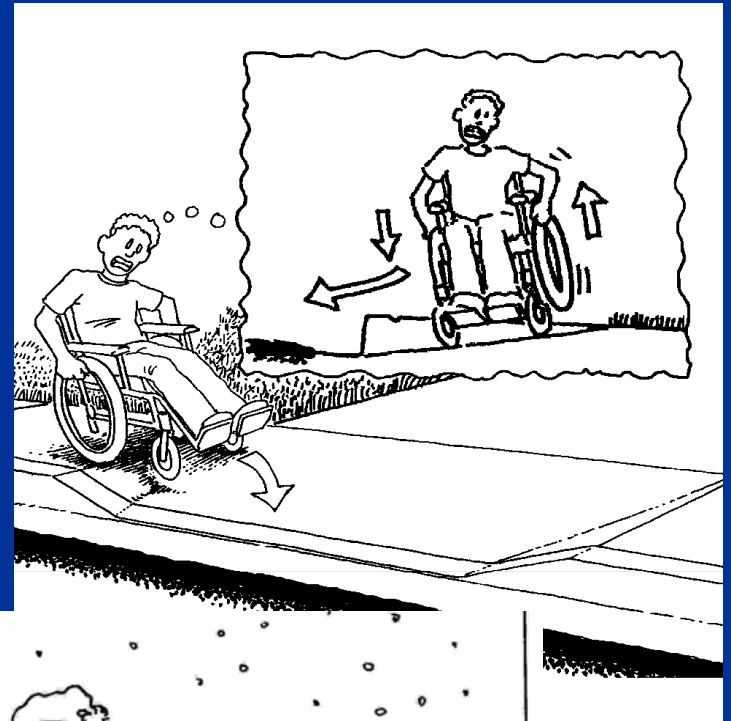
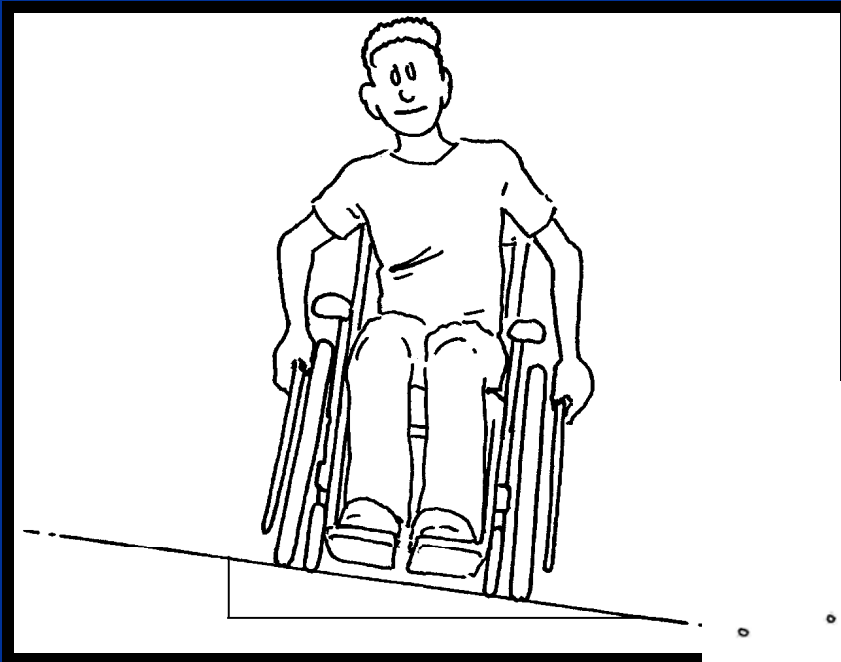


NEW CONSTRUCTION



ALTERATION

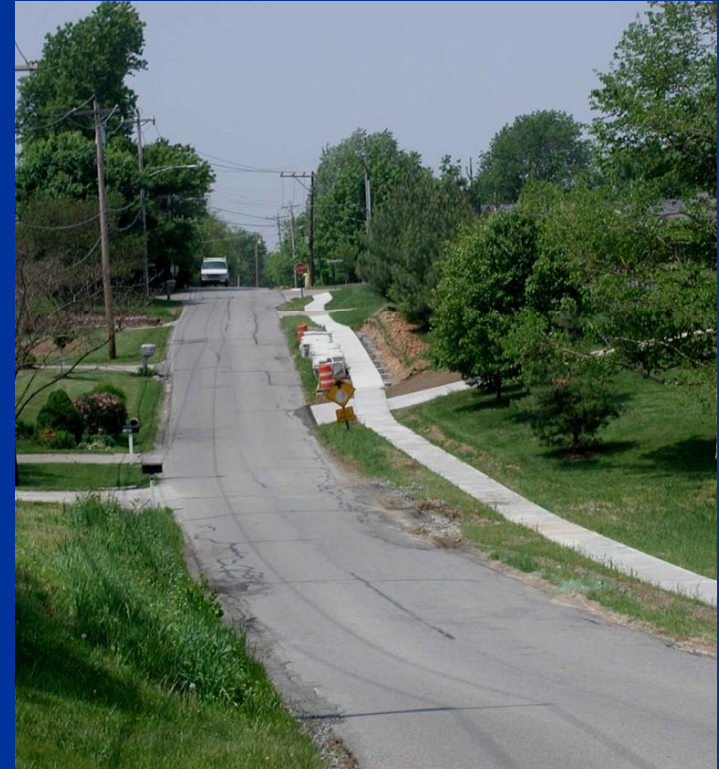
# PAR – Cross Slope



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# PAR – Running Slope

- The running slope of the PAR may match, BUT NOT EXCEED, that of the adjacent roadway.



# PAR - Intersection

- Where two directions of travel intersect at corners (or where a turn must be made), the PAR must be 'level' (2.0% in both directions) or 'tabled'.



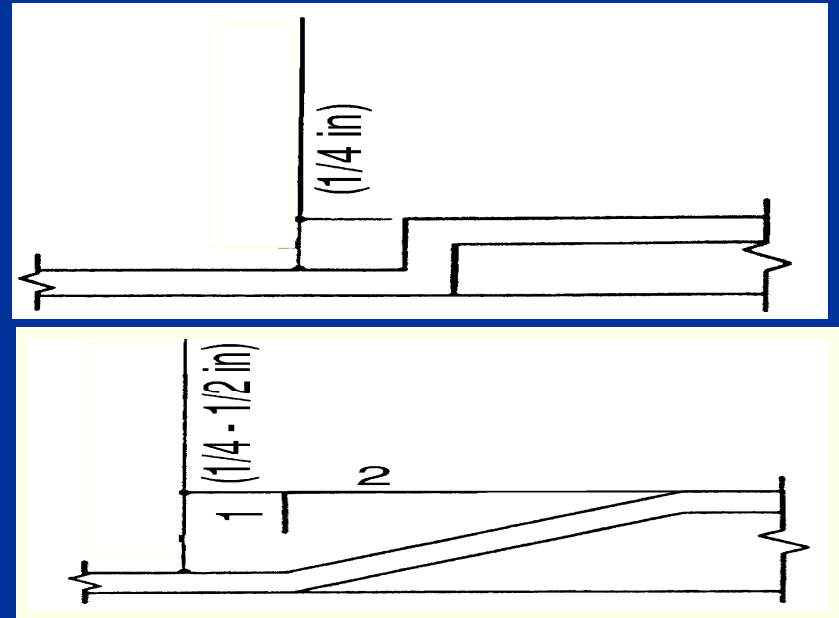
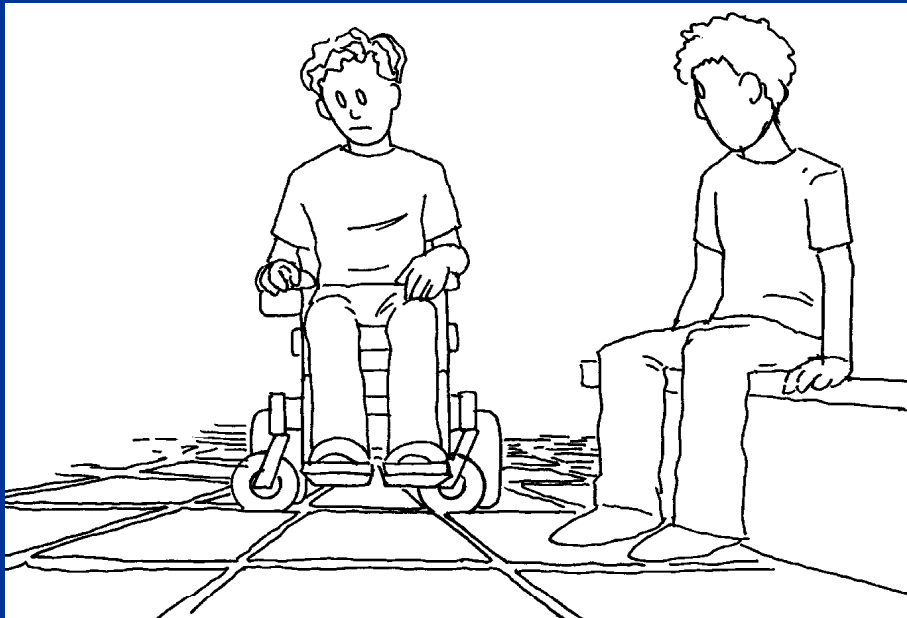
THIS ...



... NOT THIS!

# PAR – Surface

- Firm, Stable, and Slip Resistant
- 0.25" – 0.50" Vertical Difference Requires Bevel
- > 0.50" Vertical Difference Requires Ramp



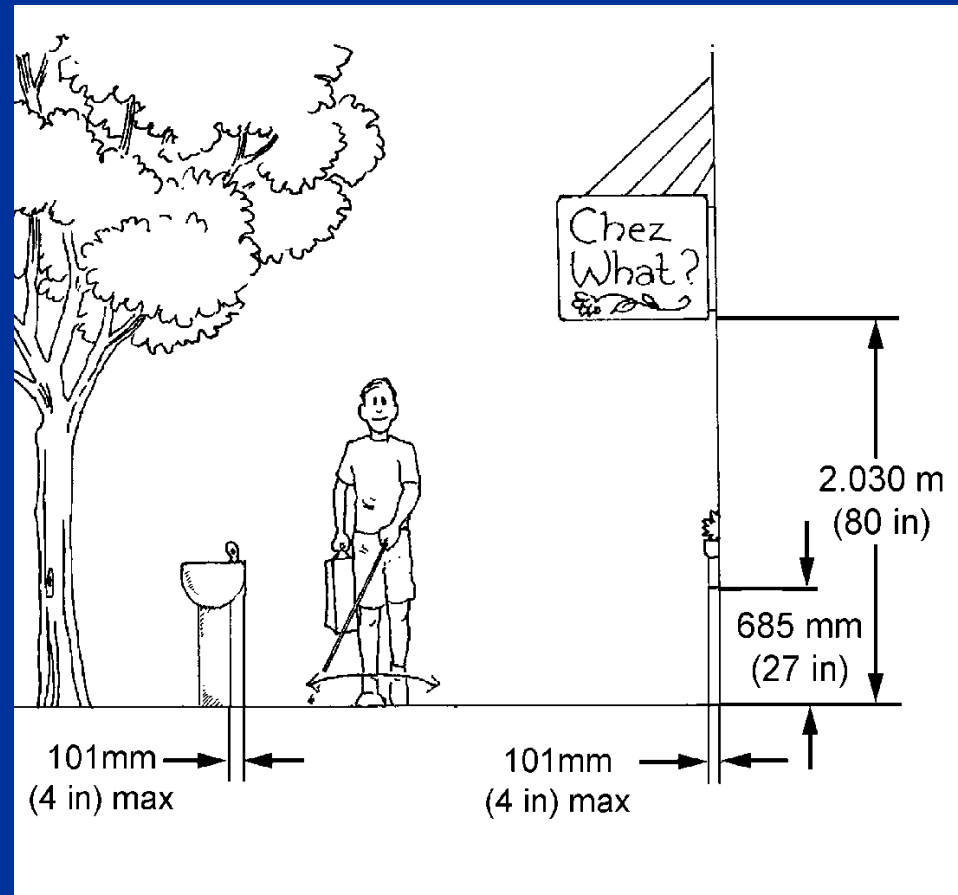
# PAR – Horizontal Openings

- 0.50" Openings
- Elongated Openings Perpendicular to Direction of Travel



# PAR - Protruding Objects

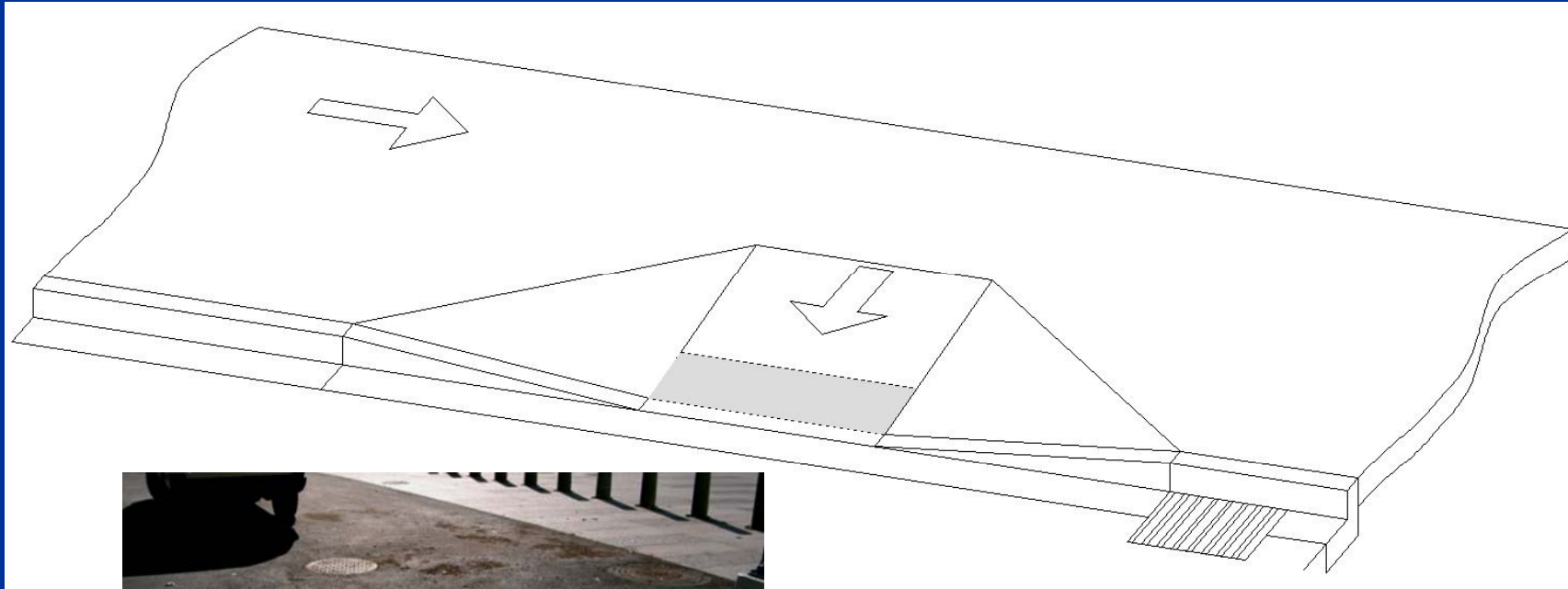
- 4" max across the whole width of the PAR
- Elements above 27" and below 80" aren't detectable.



# Curb Ramp Types

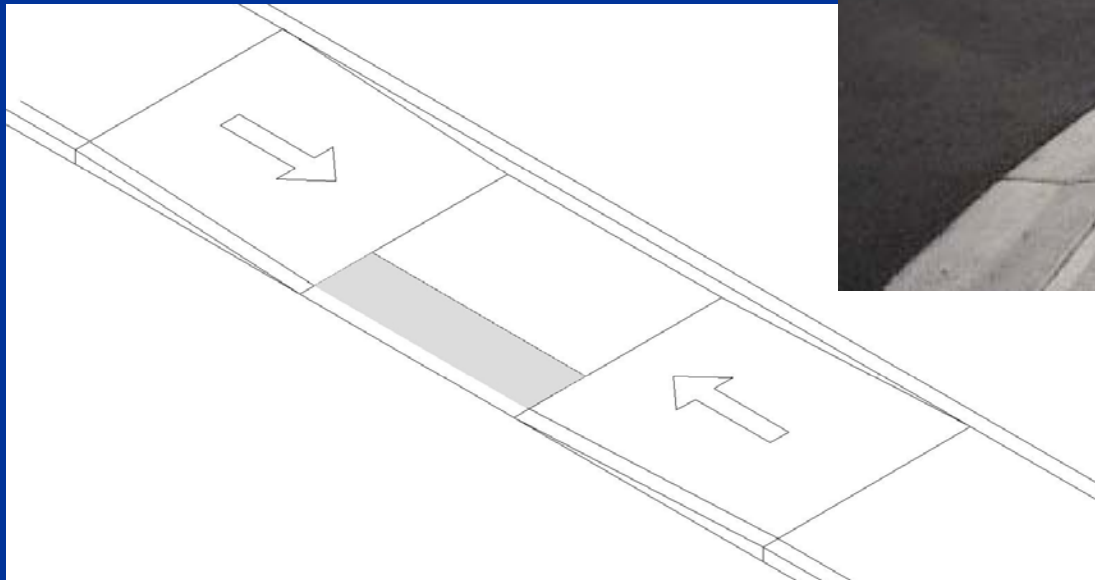
- Perpendicular to the Curb
- Parallel to the Curb
- Blended Transition

# Perpendicular Curb Ramp



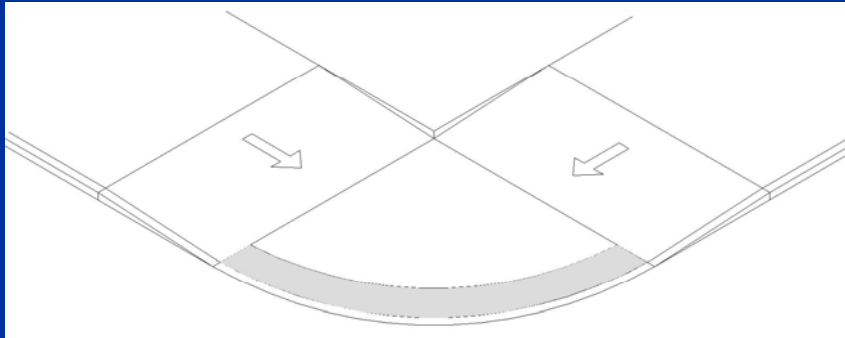
**UNITED STATES ACCESS BOARD**

# Parallel Curb Ramp

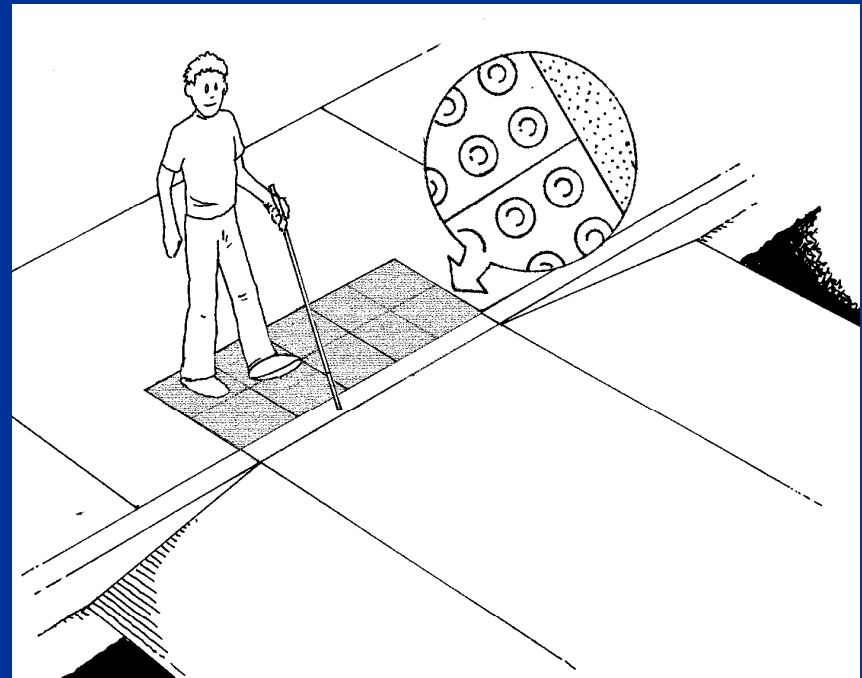


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# Blended Transition

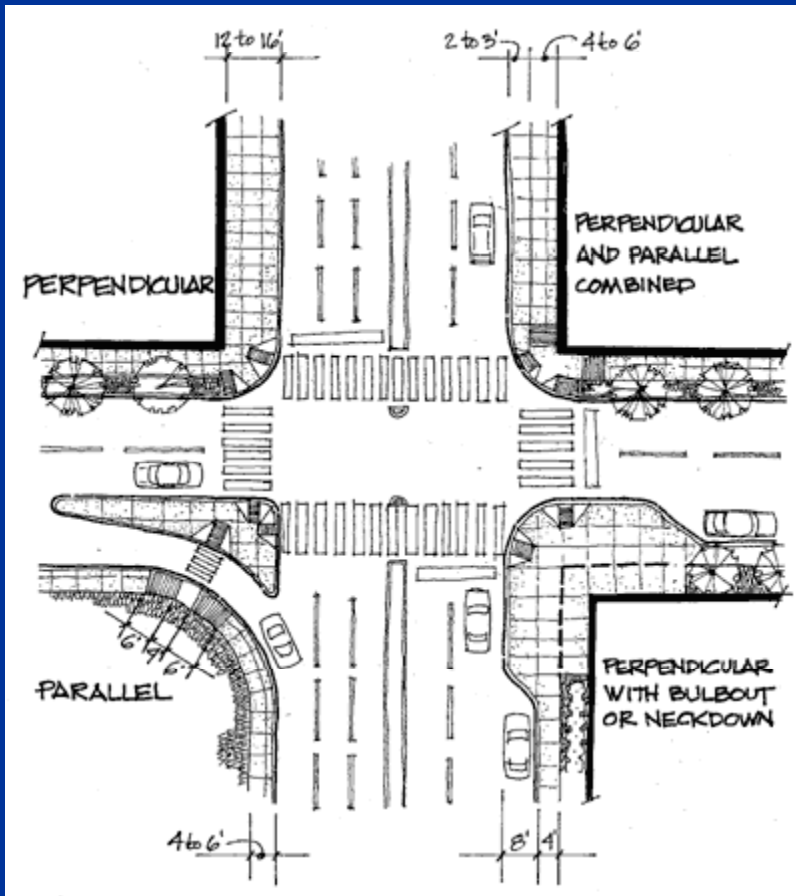


Depressed Corner



Raised Crossing

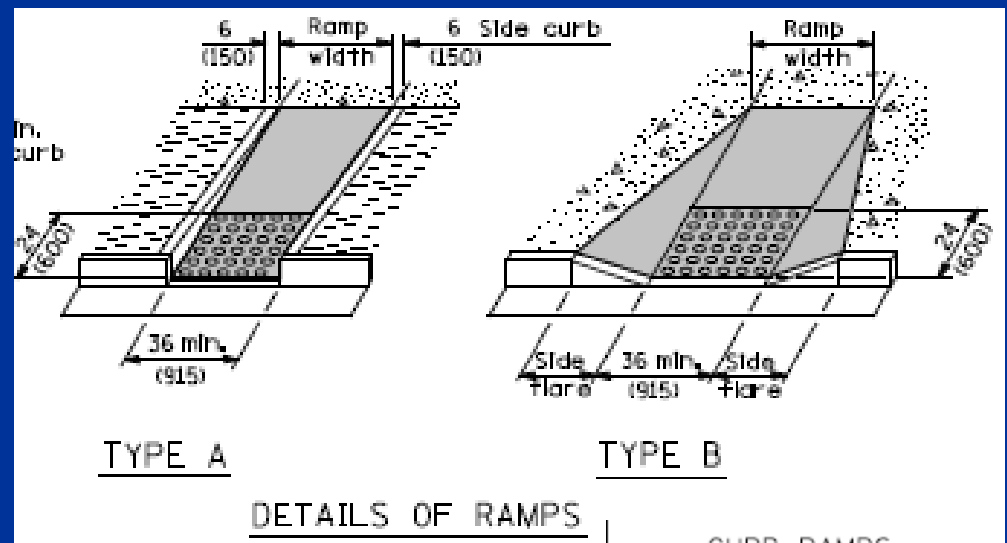
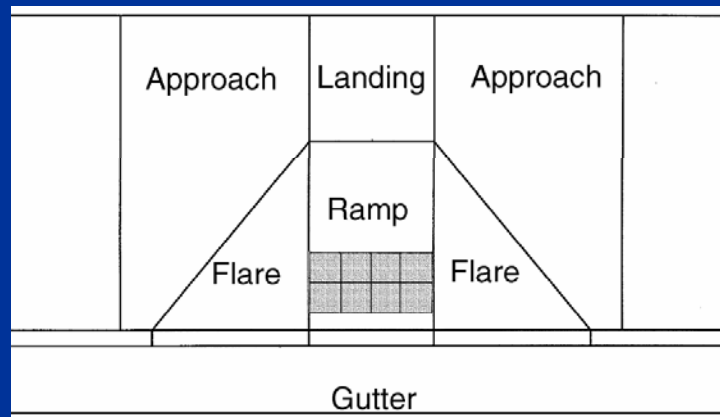
# Curb Ramp Design



- Select Best Curb Ramp or Combination
- 4' Minimum Width
- Provide 4' x 4' Turning Space
- 5% Counter Slope
- Grade Breaks Important

# Anatomy of a Curb Ramp

- The 'cookie cutter' curb ramp



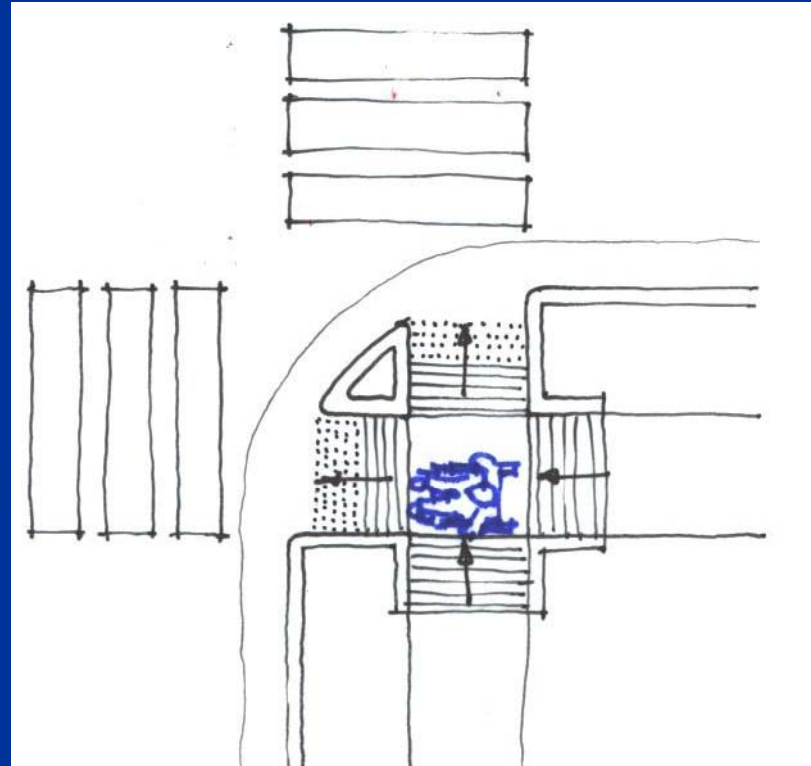
CURB RAMPS  
FOR SIDEWALKS

(Sheet 1 of 2)

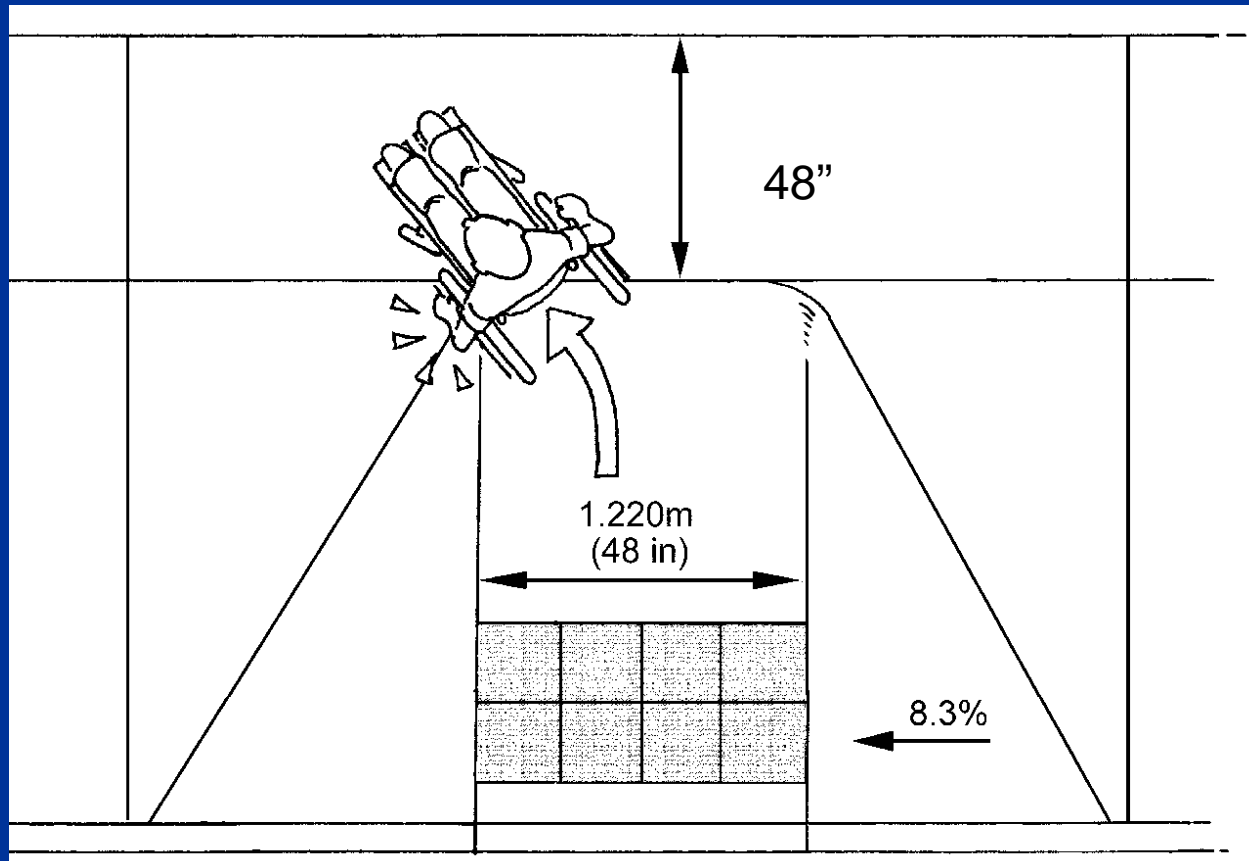
STANDARD 424001-05

# Combination Ramps

- Combined Ramps Work Well
- Sloping the sidewalk down shortens the perpendicular run to the street
- Turning space is at an intermediate level



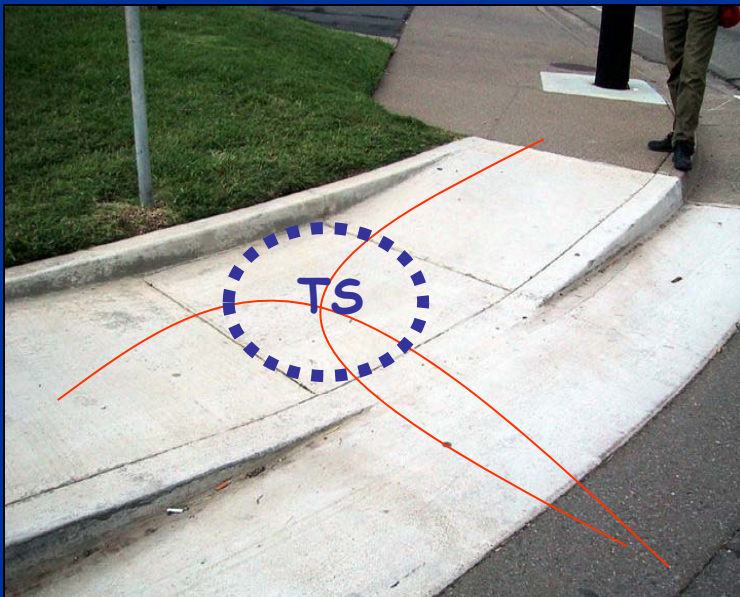
# Curb Ramps - Turning Space



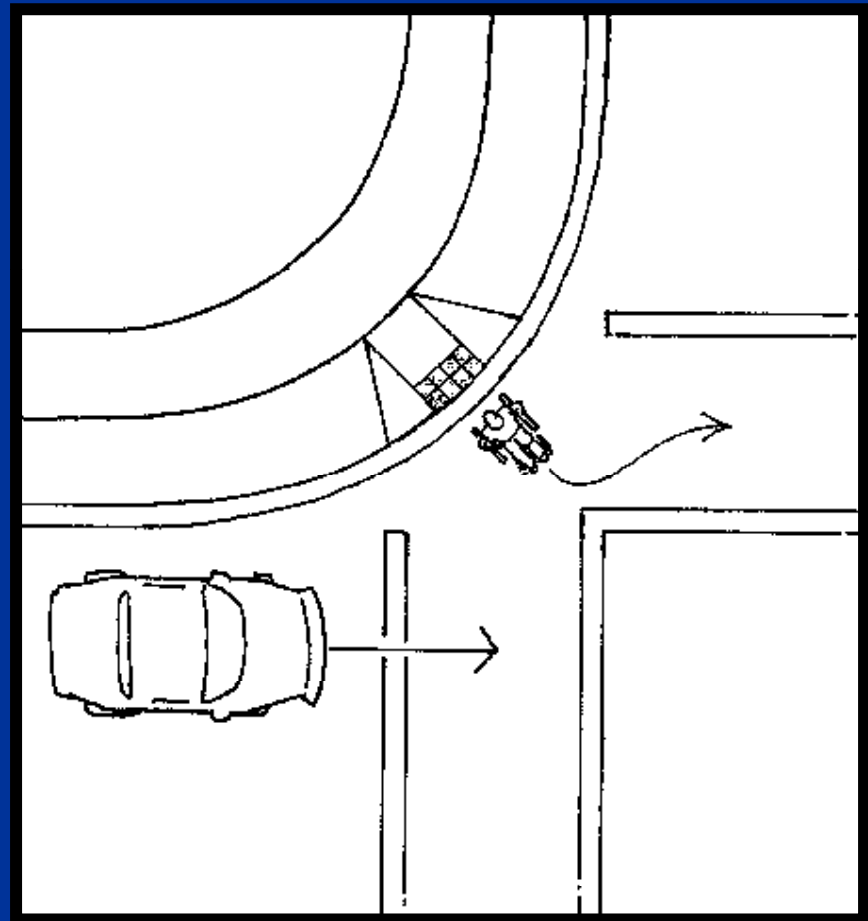
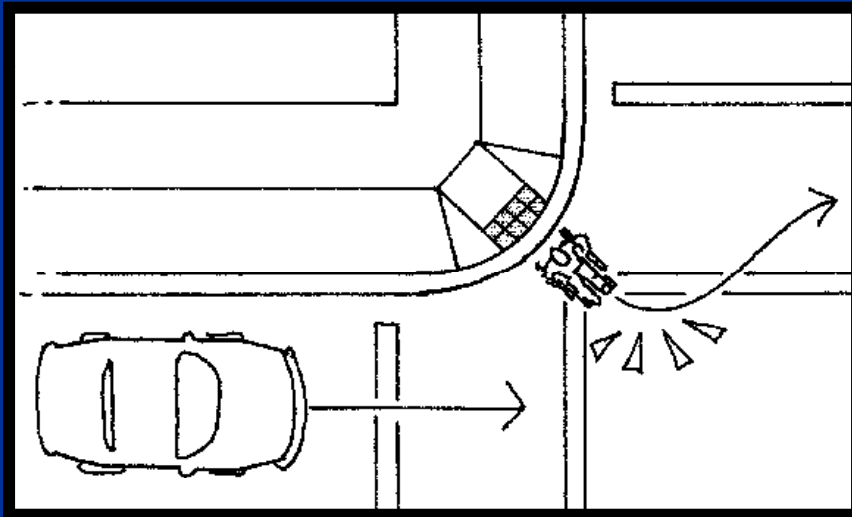
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# Curb Ramps - Turning Space

- Provide a level turning space at the top of a perpendicular ramp, at the bottom of a parallel ramp, and at corners.

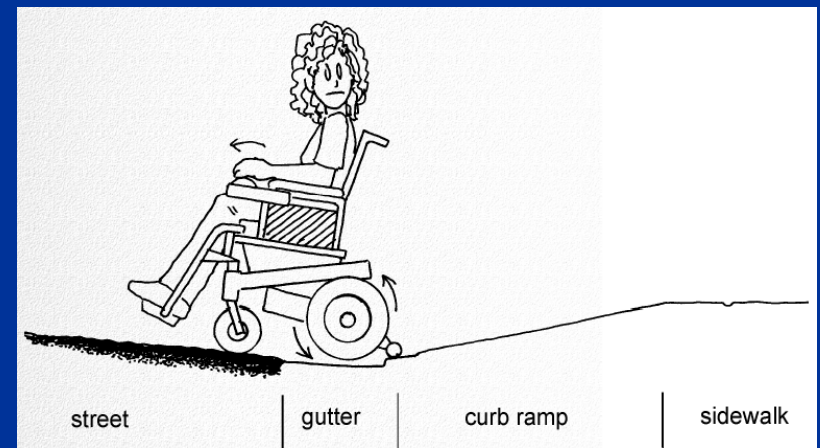
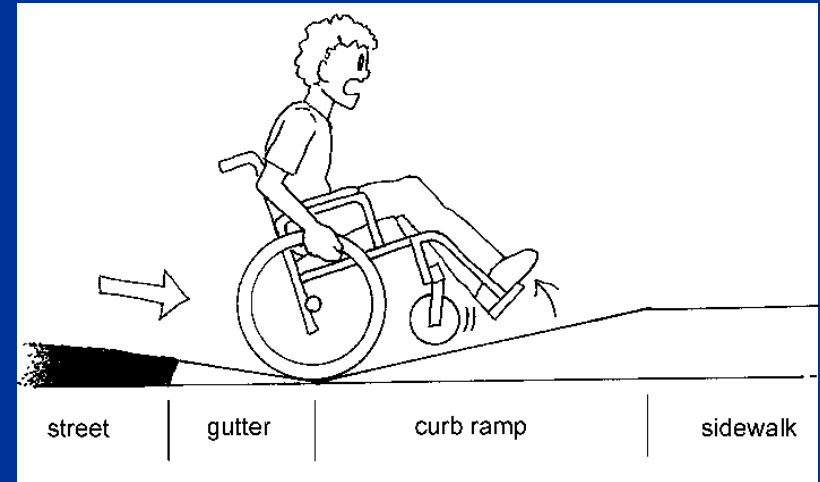
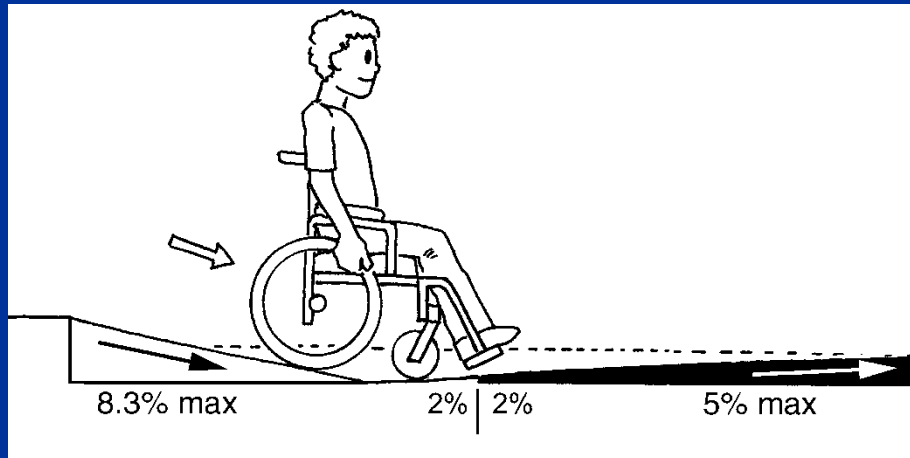


# Curb Ramps - Turning Space



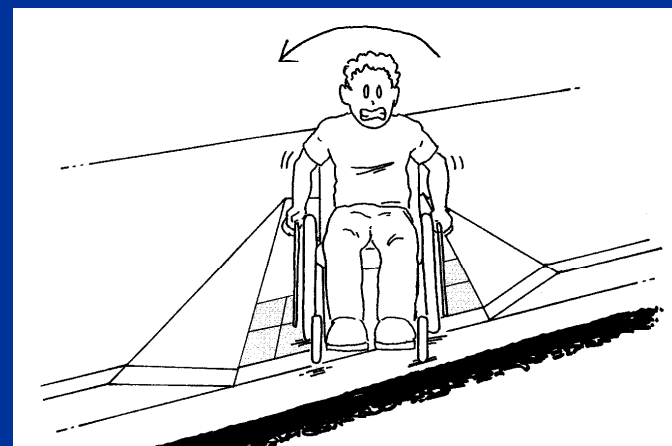
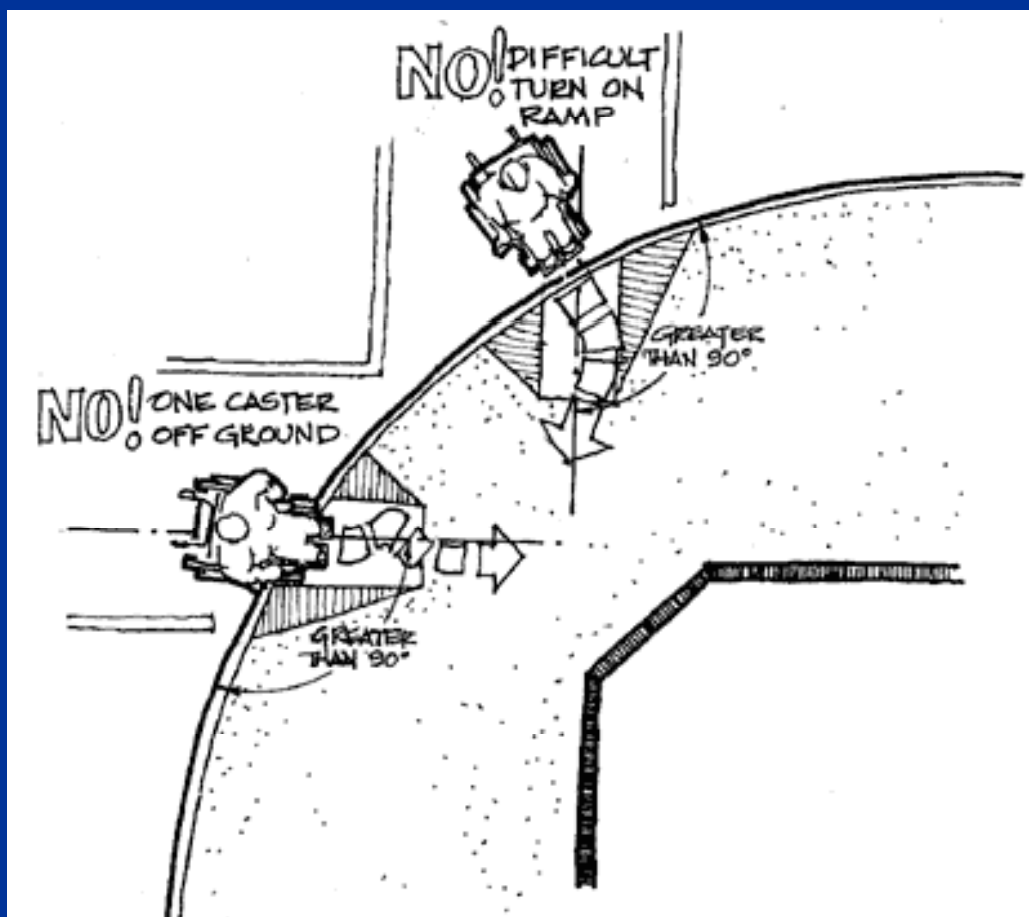
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# Curb Ramps - Counter Slope



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# Curb Ramps - Grade Break



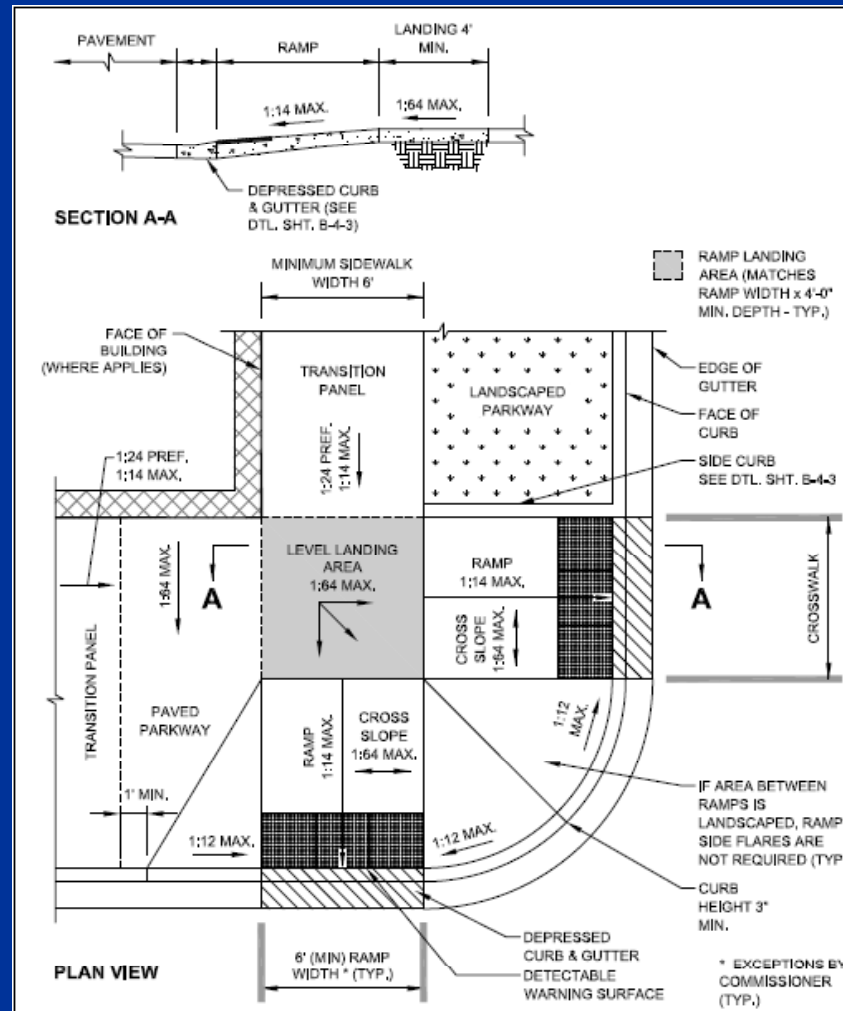
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# Curb Ramps – Ideal



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# Curb Ramps – Design Detail

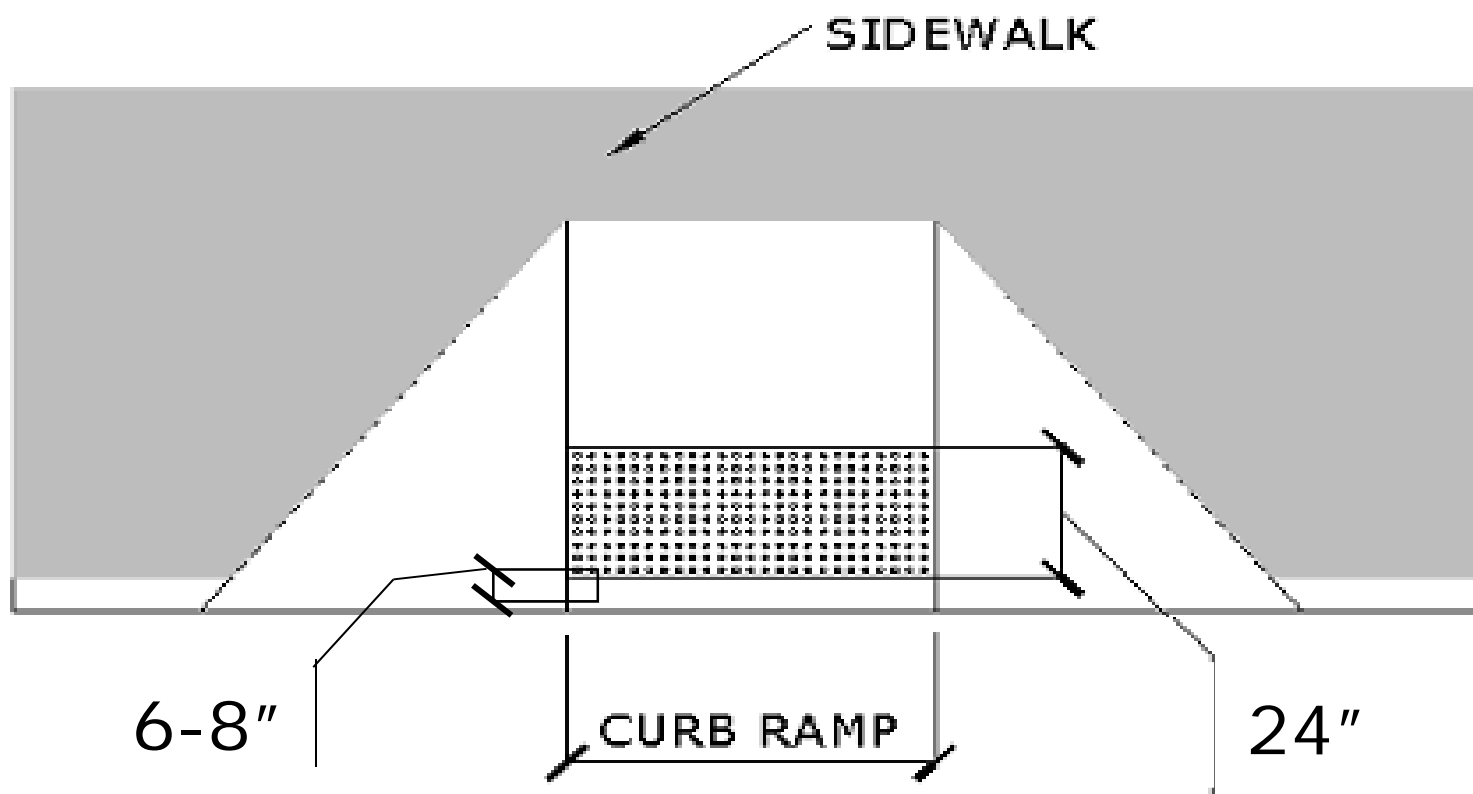


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# Detectable Warnings

- Due to their distinctive design, truncated domes are detectable by cane and underfoot
- DW Cover Entire Flush Edge or Width of Ramp
- DW must cover 2' in Direction of Travel

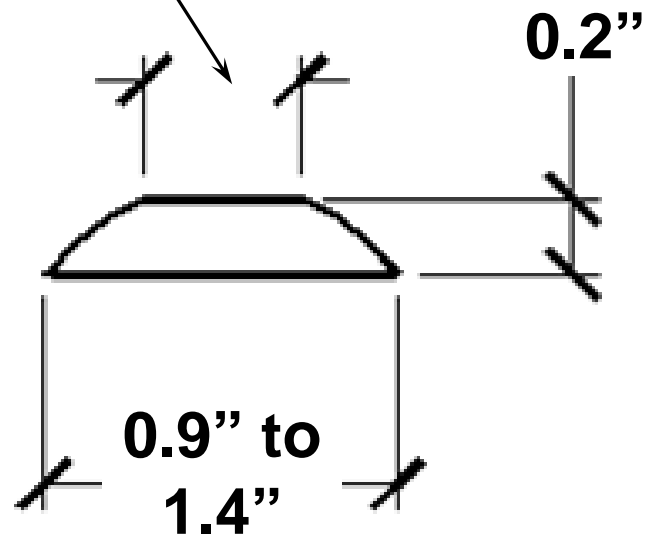
# Detectable Warnings



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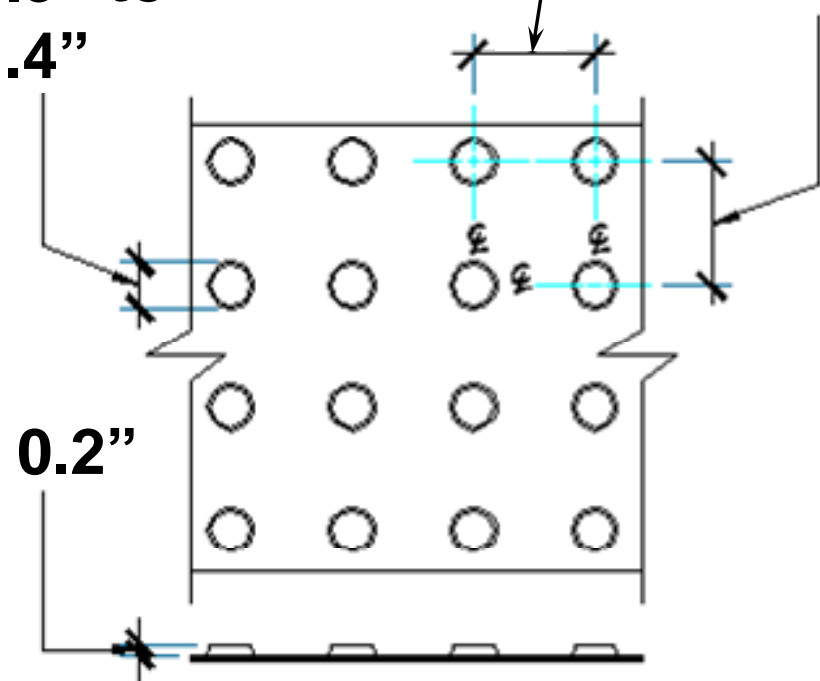
# Detectable Warnings

50% to 65%  
of base



0.9" to 1.4"

1.6" to 2.4"



# Detectable Warnings

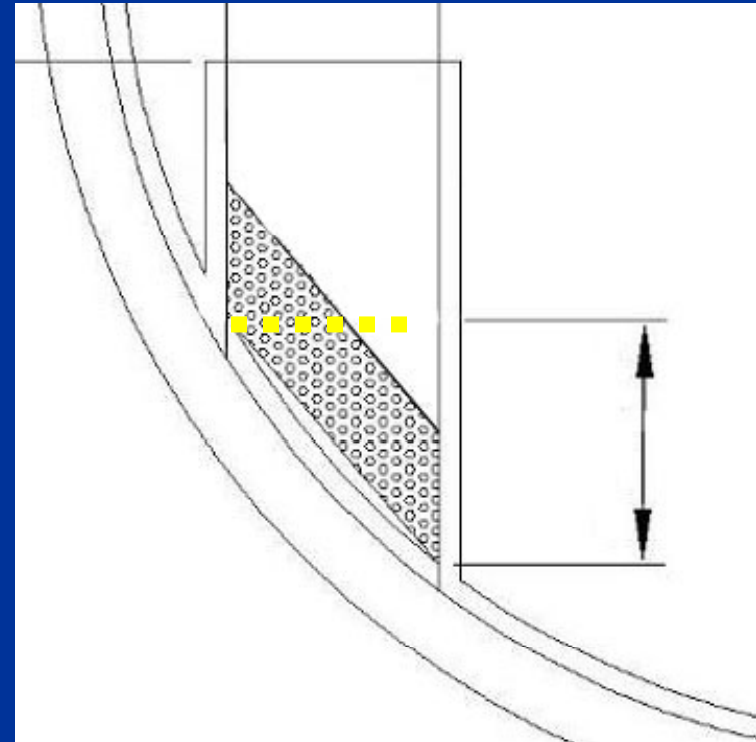
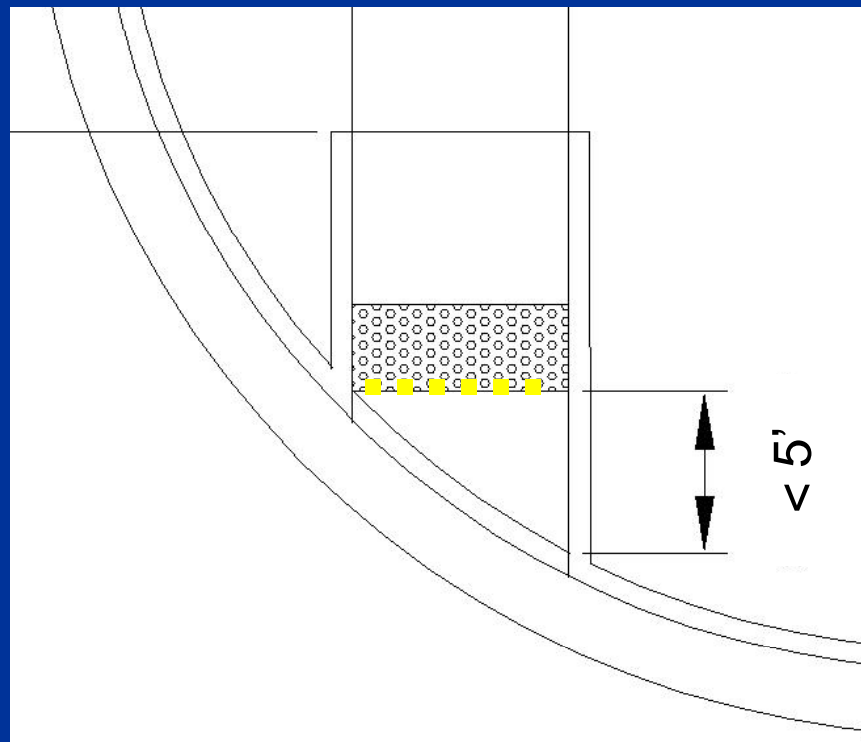


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# Detectable Warnings

- For Perpendicular Curb Ramps:
  - Place DW on curb ramp at grade break if level landing at bottom of ramp is less than 5' deep
  - Place DW on bottom landing if landing is more than 5' deep at any point

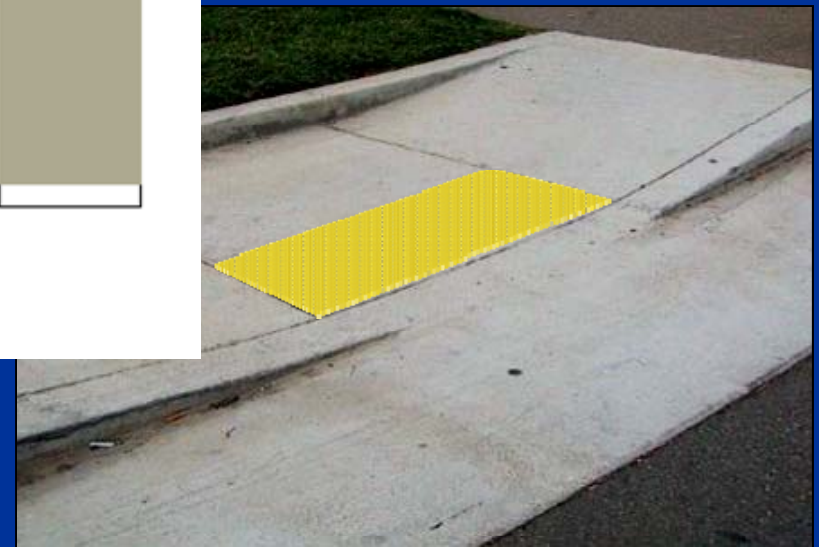
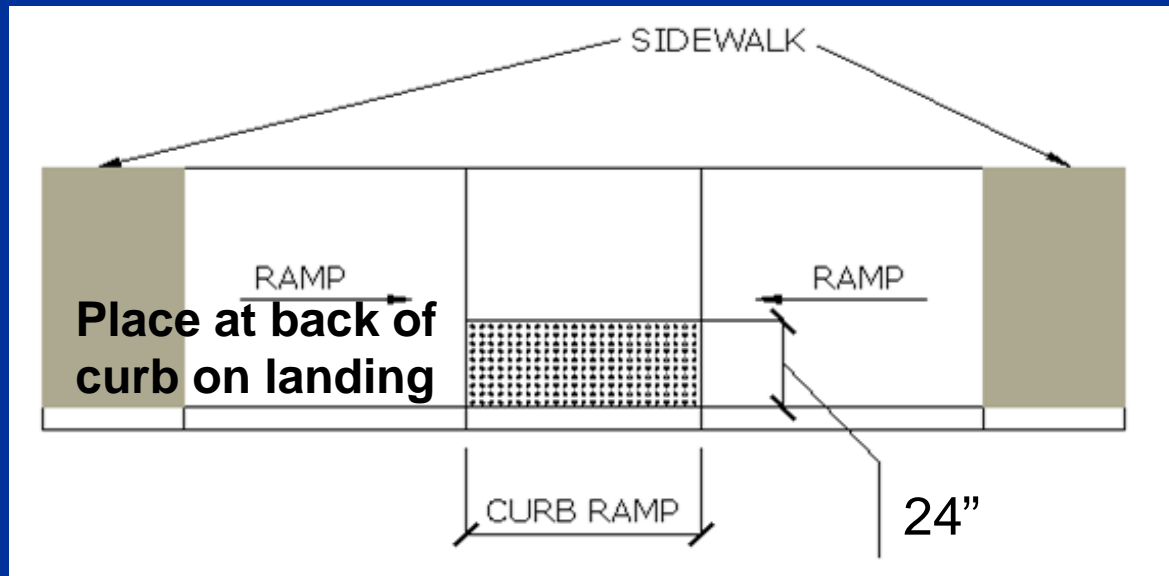
# Detectable Warnings



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# Detectable Warnings

- For Parallel Curb Ramps:



# Detectable Warnings

- Required at Medians.



# Pedestrian Crossings

- Markings Required by MUTCD
- Shorten Crossings
  - Refuge Islands
  - Median Cut
  - Bulb Outs
- APS Required for Signals
- Level (2.0% Cross Slope; 5.0% Crown)

## **Exhibit C**

# East Peoria Sidewalk Plan

## Final Survey Results

### Sidewalk Survey Results

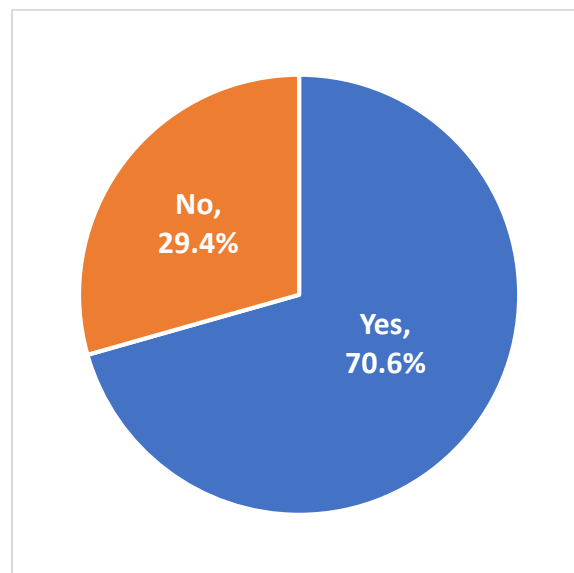
1. **Knowing that everything costs money, and that there is a limited amount of money, priorities must be set. Please rank the value of sidewalks in East Peoria compared to police, fire, and roadway funding. (1 is highest, 4 is lowest)**

This question asked respondents to prioritize, from most important (1) to least important (4), funding for Police services, Fire services, Roadway maintenance/construction, and sidewalks. Below is a summary of the responses.

Rank	Service	Avg Response
1	Police	2.23
2	Fire	2.30
3	Roadways	2.47
4	Sidewalks	3.00

Unsurprisingly, Police and Fire funding were respondents' greatest priorities, followed by Roadways and Sidewalks. More than half of respondents listed Sidewalks as their lowest priority compared to the other three.

2. **Would you like to see more funding put toward sidewalks?**



Despite their low priority compared to "essential services" and roadways, over 70% of respondents indicated that they would like to see more funding put toward sidewalks.

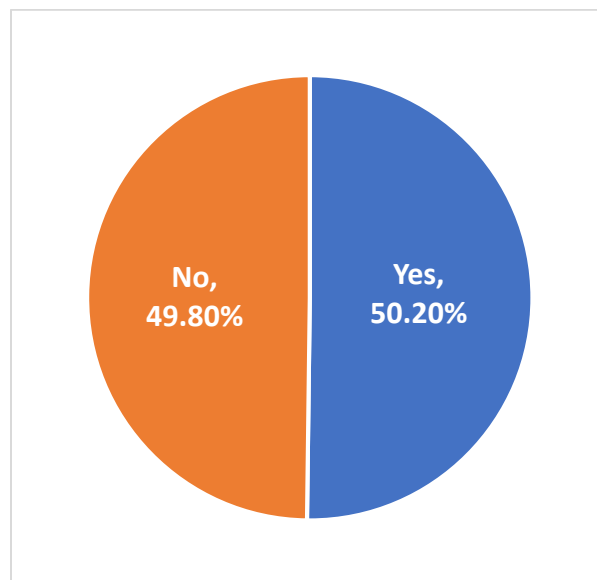
3. Assuming grants were not available, where should funding for sidewalks come from? Please rank. (1 is highest, 4 is lowest)

Money must come from somewhere, and this question asked respondents where sidewalk funding should come from, either from police funding, fire funding, roadway funding, or a tax increase.

	Funding Source	Avg Rank
1	Roadway	2.13
2	Police	2.41
3	Fire	2.62
4	Tax Increase	2.84

Based on the provided options, Roadway funding was identified as the most acceptable option for improving/building sidewalks. Police and fire proved unpopular sources, the but the least acceptable option for most respondents was a tax increase.

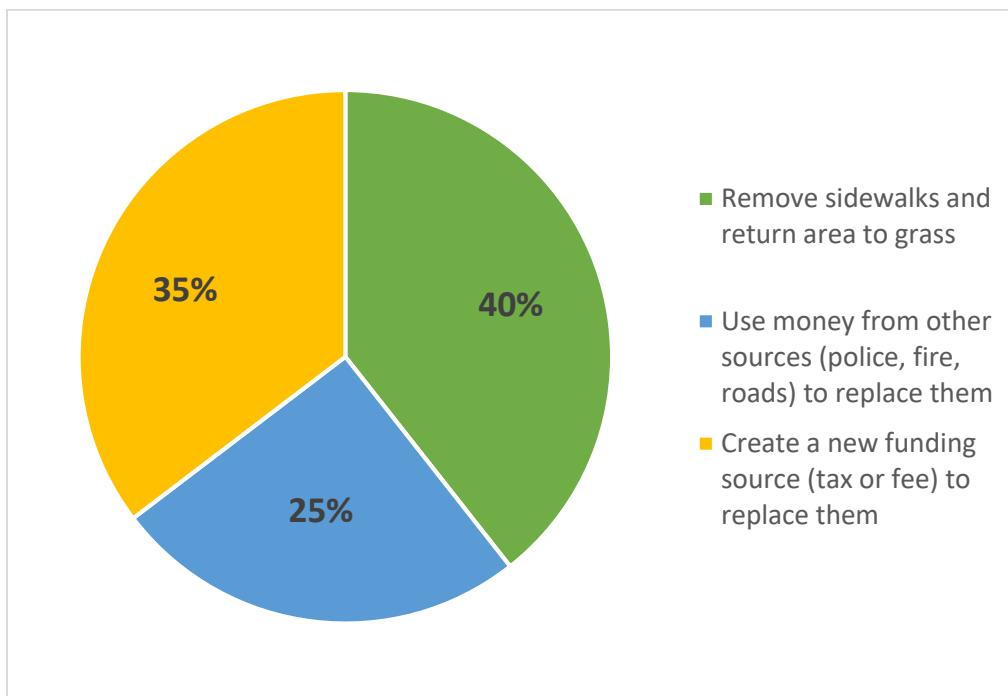
4. Whenever disturbance of an existing sidewalk occurs or when a roadway gets new blacktop, sidewalk ramps must be improved to meet current ADA standards. All existing sidewalks can remain as-is. Should money be spent to make all sidewalks ADA compatible, even if not required?



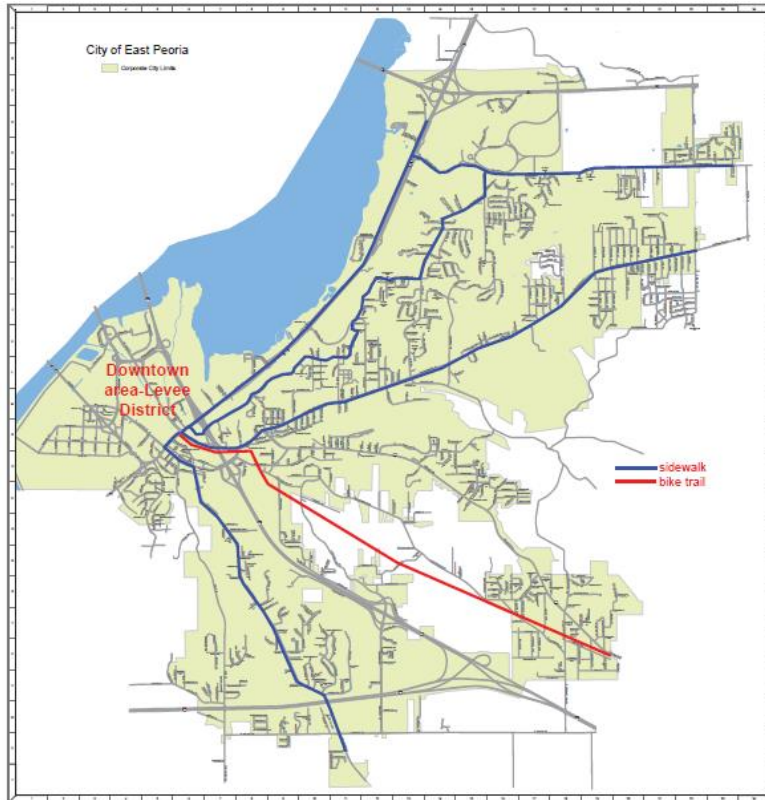
Respondents were split almost perfectly by this question.



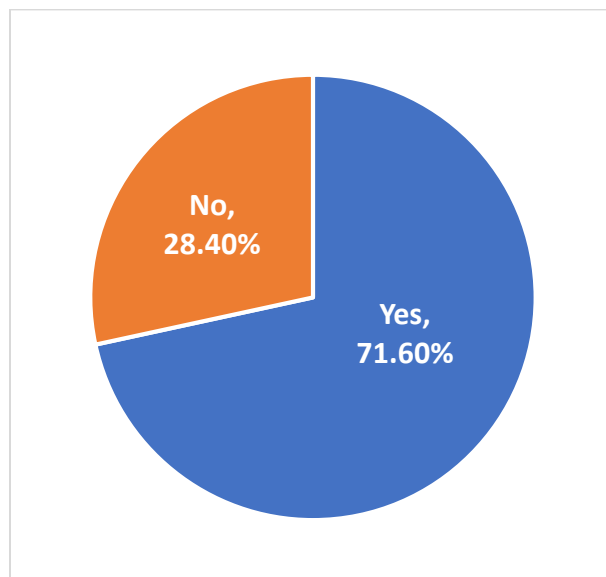
5. Deteriorated sidewalks (see image above) may be a liability for the City and the taxpayers. For sidewalks that are deteriorated or not ADA compliant, would you like to see the City...



In the case of deteriorated sidewalks, 6 out of 10 respondents support replacing them, either by utilizing existing revenue sources or creating a new one. The remaining 4 out of 10 respondents would see destroyed sidewalks simply returned to grass, indicating that they do not believe that sidewalks provide appropriate value compared to their cost.



6. Would you be in favor if the City removed all deteriorated sidewalks and planned major sidewalk improvements, installing pedestrian facilities along Springfield Rd, Fondulac Dr, Rt 116, and Meadows Ave as well as improving the pedestrian routes along Rt 8? Please see the map above.

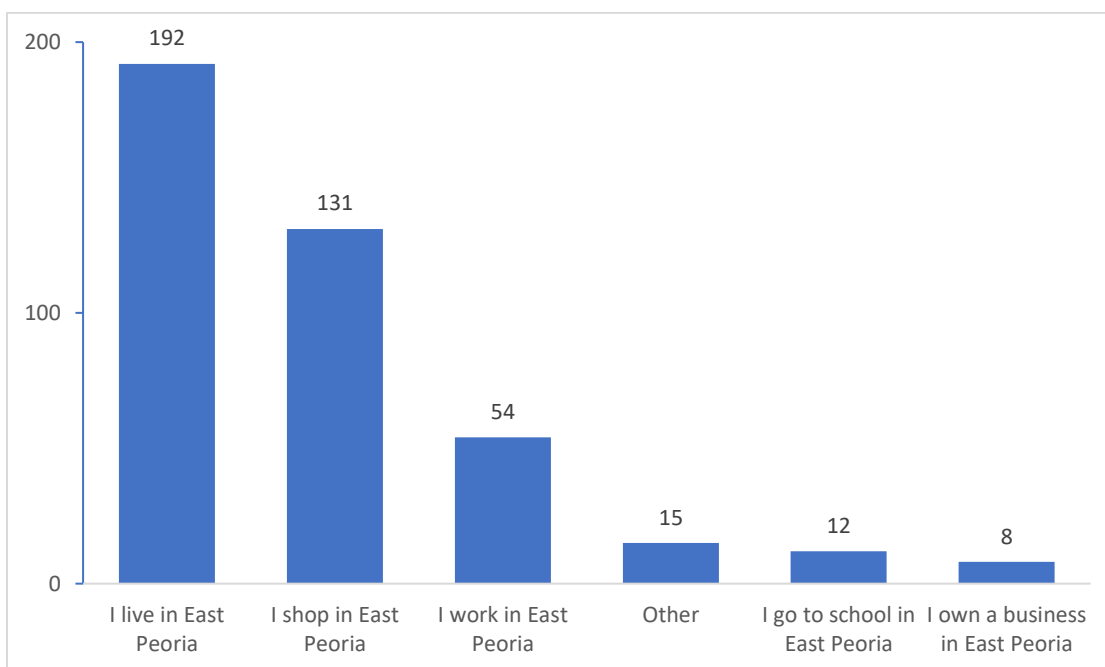


Nearly three-quarters of respondents were in favor of the “wheel and spokes” sidewalk vision shown in the map above. The proposed strategy could limit pedestrian accessibility within neighborhoods, but would increase mobility for those wishing to walk or bicycle toward the City’s core.

## Demographic Question

The survey requested only one demographic question, asking about the respondent's relationship to East Peoria.

### 7. Which of these below describe you? Select all that apply.



Almost all of the 201 respondents to the survey were City residents, more than half shopped in the City, and approximately one quarter of them worked or went to school in East Peoria. Most of the "Other" respondents referred to friends or relatives that lived in the City.

## Open Ended Question

The end of the survey invited respondents to write any additional comments or suggestions they had about East Peoria's sidewalk system (or the survey itself) in the provided field. Many commenters spoke about specific streets, intersections, or locations that they believe would benefit from sidewalks or from better sidewalks. All 84 Additional Comments are listed on the following page(s) and have not been edited for relevance or content.

### 8. Do you have any additional comments about the City of East Peoria's sidewalk network?

Sidewalks are far more valuable to resident than the time and money put into development efforts or increased city staffing expenses. (2)

No (2)

Our property taxes are already incredibly high. I highly oppose tax increase for sidewalks

Sidewalks near schools should receive the highest priority.

It's insane that students living 500yds from school are bussed because sidewalks aren't present. (I know this crosses government bodies)

I have noticed often in surrounding cities a portion of roadway being dedicated to bicycle use, this leaves sidewalks for pedestrian use only, I believe that helps with maintaining the sidewalk, slowing deterioration from heavy bicycle use.

I think that Highview Road between Armstrong and Regal Lane need to have a sidewalk. With the amount of traffic/people speeding/narrow roads, there should be some safe pathways instead of using the yards of houses/the radio tower.

Fix the roads first!

Springfield Rd needs sidewalks down the entirety of the street all the way from Washington street to muller to help keep the terrible traffic away from our children and daily walkers that occur related to the narrow roadway. The Springfield Rd community cannot walk safely as it is today. Please consider.

Having sidewalks is great for those who choose not to drive or those who can't drive. People who walk deserve safe passages as well.

Edgewood Subdivision needs sidewalks and Armstrong school needs the walking path repaired and would benefit from a sidewalk along Highview Rd/Illini on one side of the road for safe walking/biking to school for kids.

Would love sidewalks on highview and illini drive for the walkers and runners

Pekin Ave!! needs sidewalks too.....

Fondulac Drive needs sidewalks. This is a popular walking area, but cars drive too fast and there isn't much shoulder area for pedestrians. I think this would be a great asset to the community.

they need improvement

Add sidewalks to residential neighborhoods

Contracted companies, ie itv3 needs to repair the curbs and sidewalks that they ruin while installing fiber.

Love sidewalks, but we just can't afford them right now. Too much is spent on fire & police and too much DEBT!

We need more sidewalks throughout the City.

Need sidewalks on illini and highview. Lots of walkers who are at times hard to see in early morning.

I think there are sidewalks that aren't used that can be removed. Then there are sidewalks that are needed that are not on this map duh as residential areas and by elementary schools.

We also have curbs that need repaired and kids playing in the streets because there are no side walks

This survey is crap. There are other options besides the ones listed. Nobody in their right minds are going to take away from police and fire and EP taxes are already high enough. Try again. This time with effort

May I repair the medium break up of the ramp/sidewalk leading to my driveway?

We have other priorities. The sidewalks that exist are in bad shape. We either need to tear out and replace in addition to new or just tear out.

I have lived on Fondulac Dr for almost 10 years. My wife and I have discussed numerous times how unfortunate it is that one of the nicest areas in the city has no safe travel path. I personally would be willing to pay a higher tax for a safe place for kids to ride their bikes.

I walk often with my dog and haven't seen a major issue. I'd say keep up with the trail and it'll be fine. Or even paint bike paths on the roadways.

Please fix the roads before anything. They are bad. Springfield Hill is terrible.

Sidewalk needed on Highview

I would love to see the city take pride in our town instead of it looking run down like Peoria. We're better than this guys.

We need sidewalks on Highview Road. This survey is an example of Firemen First. This means whenever a city is faced with a challenge, the threat of losing fire protection is implied. .

These seemed like loaded questions. Is that really the only places in which to get money for sidewalks? Seems like you picked the most important items that needed funding and put them up against sidewalks with the only other option being a tax hike. If these really are the only areas to get funding for some legal reason I wish that was explained. Otherwise this seems like you directed participants to a certain answer so later you can say "This is what the people want."

It would be nice to get sidewalks on Pleasant Hill of some sort. Lots of people bike and walk up the road to get to the bike trail and it's not the safest road to be on.

The main, high traffic and winding roads should have sidewalks. I live on Juniper Lane and am a runner. It is neither easy nor safe to run on most of Highview and Oakwood. It would be nice to have a path that stretches up Illini, Highview and Oakwood to ICC.

Sidewalks would make our kids safer

As a teacher in district 309, I would love to stay in the community. One of the biggest complaints to my husband is the lack of sidewalks. I also think Highview/Illini is extremely busy and there are always joggers, walkers, and people like me with strollers. I'd love to see sidewalks here, too.

Many areas don't even have sidewalks!

I like the map and plan for sidewalks!

Pensions are a thing of the past, but the city still offers them to the police and fire departments. No. I'm sick of my constant tax increases. Where is our money going? Someone needs to take a cut and it's not our roads.

Highview Rd needs better pedestrian protection

Highview Rd needs sidewalks

I have always felt that East Peoria isn't incredibly "walker/runner" friendly in areas near ICC/Fondulac Dr/Meadows Ave where you typically see foot traffic using busy roadways or walking in ditches. I understand that all improvements come at a cost, but I think this would be a nice addition to our great city.

No need to add sidewalk where there are none

If you remove sidewalks, pedestrians must have a safe place to walk on the road. Foot and bike traffic is important and healthy. Plus we need to protect our children from vehicular traffic. I would support lowering speed limits on road with pedestrian/bicycle traffic and expect greater law enforcement on said roads.

Arnold Road needs them. Any road with a school on it should have a sidewalk.

I personally am more concerned about the lack of sidewalks in our neighborhood (Edgewood Subdivision), and would rather have sidewalks in the neighborhood than in commercial areas.

Pekin Ave desperately needs sidewalks since the bus doesn't run through here anymore.

The Highview RD Simon Dr neighborhood needs sidewalks badly. People drive too fast along the windy roads within the neighborhood. People are uncomfortable letting their kids out in the neighborhood because of the traffic.

There aren't enough sidewalks in the area around Lowe's and Walmart. There should be benches at bus stops in the Levy District.

I would rather see our roads repaired first.

Why not take the money from the city officials budget instead of comparing it to police and fire? The survey is biased and I would rather see police and fire have higher budgets than see them get funding taken away AGAIN to something that could come out somewhere else. Don't make it seem like those are the only places money sits in this city.

I would like to see sidewalks where there currently aren't any, e.g. Fondulac Dr

This is an extremely biased survey. Police, fire and roads are not the only areas on which taxes are spent or where cuts could be made. And yet those three options are obviously the ones chosen to elicit the response you want that sidewalks in comparison aren't that important. In reality, governments could find money to eventually support all worthy projects if they focused on efficiency and cutting unnecessary administrative costs. Don't pretend that everything is an either/or. There is often more than one way up a mountain, and there is surely a compromise that can be reached to ensure adequate and safe sidewalks. Also, you cannot answer the question about the deteriorated/non-ADA sidewalks as an all or none; it depends on the location, the amount of use, and obviously the personal opinions of the

people most affected in each specific neighborhood. I might not care about the sidewalk in that picture, but a handicapped person living in that house surely would.

The deterioration in the sidewalk above is not totally the city's fault. Shouldn't the homeowner bear some responsibility for upkeep? Partner with the citizens. Require upkeep.

Highview Rd and Illini need sidewalks. Tons of people walking on the dilapidated roads

I know you want to work on sidewalks with the most traffic but there are a lot of sidewalks in neighborhoods that need repaired

East Peoria is not community friendly since families here do not have a safe place to walk in their own neighborhoods.

Previous question is not a yes/no question. Who writes these surveys? What network? New subdivisions should be required to install sidewalks. Homeowner responsibility for maintenance. What about Highview Road. So many walkers, runners, bikes on it. Sidewalks are very important. One reason people move to Washington and Morton. Quality of life

It is crazy to me that there are not sidewalks along vast portions of the local bus routes. Even on hugely busy roads people have to walk on the road or on grass verges to get to bus stops, even on steep hills (i.e. near ICC). This is so dangerous for both pedestrians and drivers alike. I also think you are currently exaggerating in calling anything a "Sidewalk Network", the sidewalks in East Peoria are so few and far between, starting and stopping abruptly with what seems little to no thought or logic (I'm pretty sure "Networks" have to be connected).

We don't need to waste money on sidewalks.

Instead of concentrating on sidewalks where people can walk just fine, maybe concentrate on our horrible roads where cars are damaged from driving on them.

Save the pool. You are a disgrace!!

Try to get creative with funding, don't just assume the city will bear another tax. Crowdfund, pull from existing budgets, etc.

Pull funding from other budgets not listed here. Don't create another tax for the sake of taxes.

Find funding from other sources or budgets; use what you already have.

Springfield Rd could use sidewalks but only if funded by grant money and not increased taxes.

I have lived in EP for 50 years. I never had a sidewalk. Basic Road safety was and is still enough.

Would love to see sidewalks on Centennial towards Washington.

No tax increase. Enough is enough.

East Peoria needs road and street repair before spending additional funds on sidewalks

Network is an inappropriate word for how things currently are. There are almost next to nothing in terms of keeping people safe. I have seen many having to walk on the road, nearly getting hit on a

daily basis. The bus stops are also in desperate need, especially by ICC. The term dangerous isn't strong enough to how inappropriate that stop is setup and would benefit greatly from a side walk, along with the rest of East Peoria.

Sidewalks help to make America great again

Major sidewalks along busy roads should be/NEED to be cleared of snow!

I have felt unsafe several times walking down the road because there is not a sidewalk in place to keep myself or my dogs safe when we are walking together. This takes places daily on Oakwood Road. Having a sidewalk would make getting around the area much safer. Thank you.

I feel there should be sidewalks in the neighborhood by ICC & Armstrong elementary school. There are many children & students who walk to school and around the neighborhoods and it would be much safer for them if there were sidewalks

I believe the city should initially install sidewalks in all business and high traffic areas, then make fronting property owners responsible for upkeep by fees or fines

We don't have any in our subdivision and it's not save. It should be a requirement when there is a subdivision.

The hill going up Centennial Road to ICC is extremely steep. (Not sure how this could meet ADA standards) I would like to see a protected bike lane installed next to Route 24 from College Dr to the planned McClugage Bridge pedestrian/bicycle path. I would also like to see a sidewalk along Illini Dr from Washington Street to Highview Road.

I am so scared someone is going to get hit by a car walking, riding bikes on Springfield road or Fondulac Dr. There should be more enforcement of speed limits on both roads. Fondulac Dr. is park district, there should be more safety signage, speed limit signs or children at play signs on Fondulac. There are so many blind curves on Fondulac as well and tons of people walk with their dogs, and children in strollers. I hope the city seriously considers doing something before something tragic happens.

East Peoria needs sidewalks for safety and community accessibility. It is unfair to ask the residents of East Peoria to make a distinction between sidewalks and fire/police. Both need funding and should be completely separate issues. East Peoria has plenty of revenue options other than making it more expensive to live in town while granting exemptions for every business. Fondulac Drive is a very busy road and a focal point of our city yet has literally nowhere for people to walk. In order to foster a sense of community rather than a focus on businesses that do not contribute fairly to the city in which they exist, we need to provide our citizens with safe access to throughout our community.

I live on 802 Fondulac Dr. the neighborhood is getting more and more younger with children, having sidewalks is crucial up their so these kids can ride their bike or even for the older people that like to walk. It's dangerous not to have them because the cars up their drive ridiculous! We need children at play signs, sidewalks and maybe even a couple speed bumps! My family has even reconsidered moving to another location that has sidewalks! Sidewalks would be very beneficial to the neighborhood all along Fondulac dr.

I would love to see sidewalks in the Highview Road/Illini Drive area also.

We live on Fondulac Drive, on which several people walk or ride bikes. I fear for everyone's safety, specifically walking, due to the curves and speed at which some cars drive on our road. Sidewalks would be such a welcome and necessary improvement to keep our citizens safe, not to mention the children living on this road.

## **Exhibit D**

# City of East Peoria, Illinois

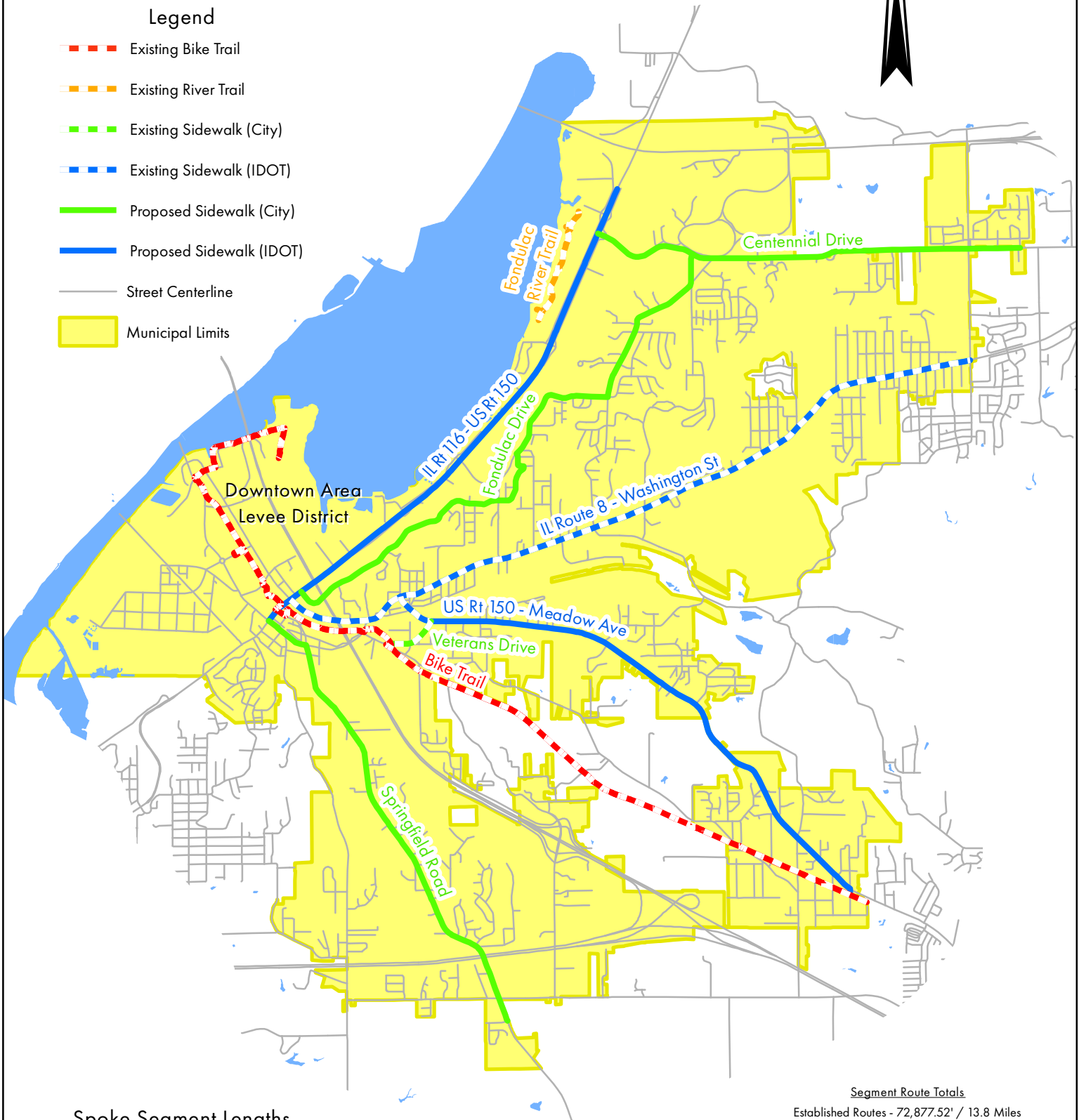
## Proposed Pedestrian Conceptual Plan

0 0.5 1 2 Miles



### Legend

- - - Existing Bike Trail
- - - Existing River Trail
- - - Existing Sidewalk (City)
- - - Existing Sidewalk (IDOT)
- Proposed Sidewalk (City)
- Proposed Sidewalk (IDOT)
- Street Centerline
- Municipal Limits



### Spoke Segment Lengths

\* Current Established Routes

\* Bike Trail - 39,219.78' / 7.43 Miles

\* Fondulac River Trail - 5,064.58' / .96 Miles

Springfield Road - 18,440.29' / 3.49 Miles

Centennial Drive - 16,555.37' / 3.14 Miles

\* IL Rt 8 - Washington St - 28,593.16' - 5.42 Miles

US Rt 150 - Meadow Ave - 22,009.47' / 4.17 Miles

### Segment Route Totals

Established Routes - 72,877.52' / 13.8 Miles

Proposed Routes - 103,839.54' / 19.67 Miles

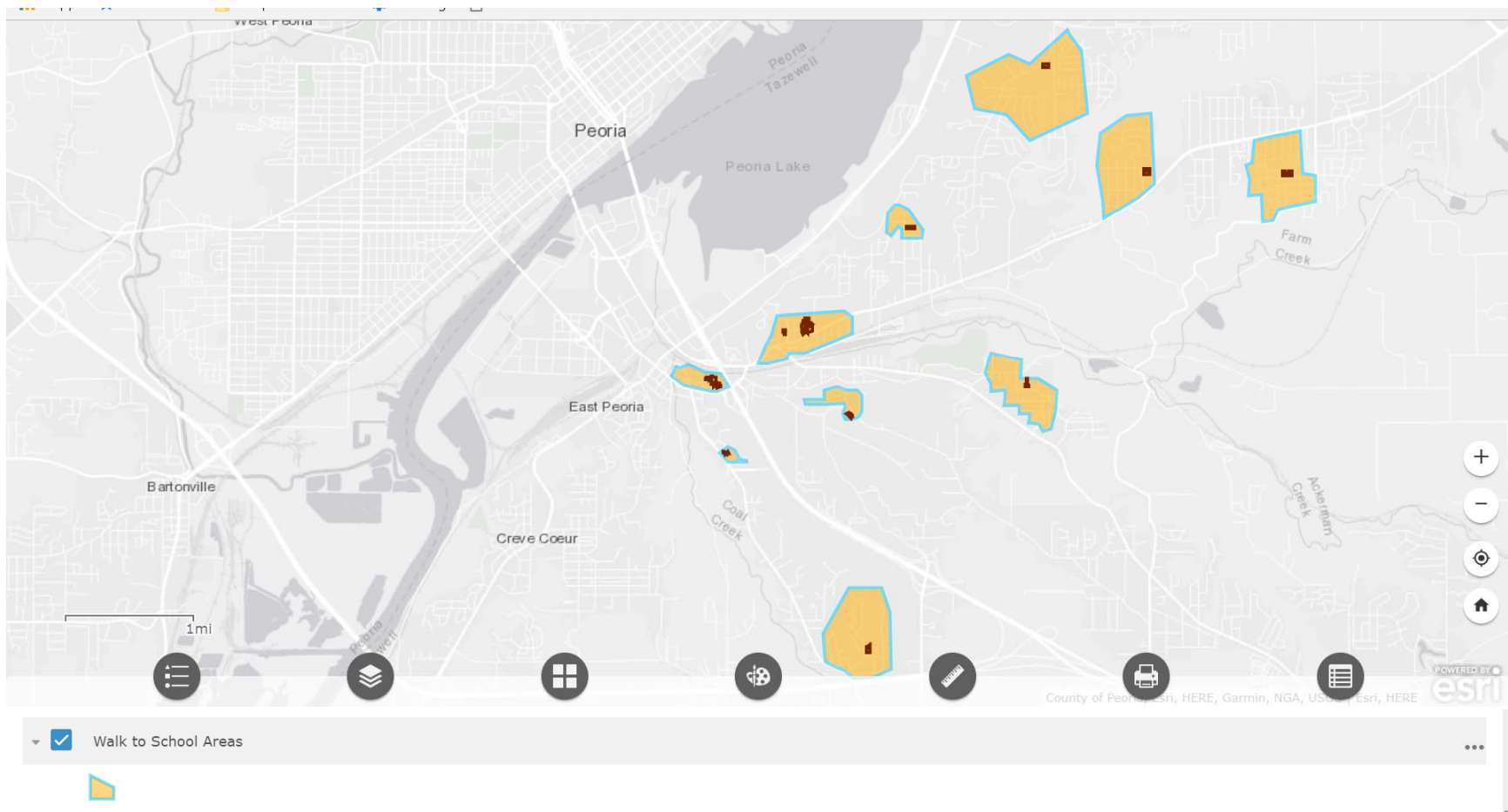
Fondulac Drive - 23,444.14' / 4.44 Miles

IL Rt 116 - US Rt 150 - 21,585.67' / 5.42 Miles

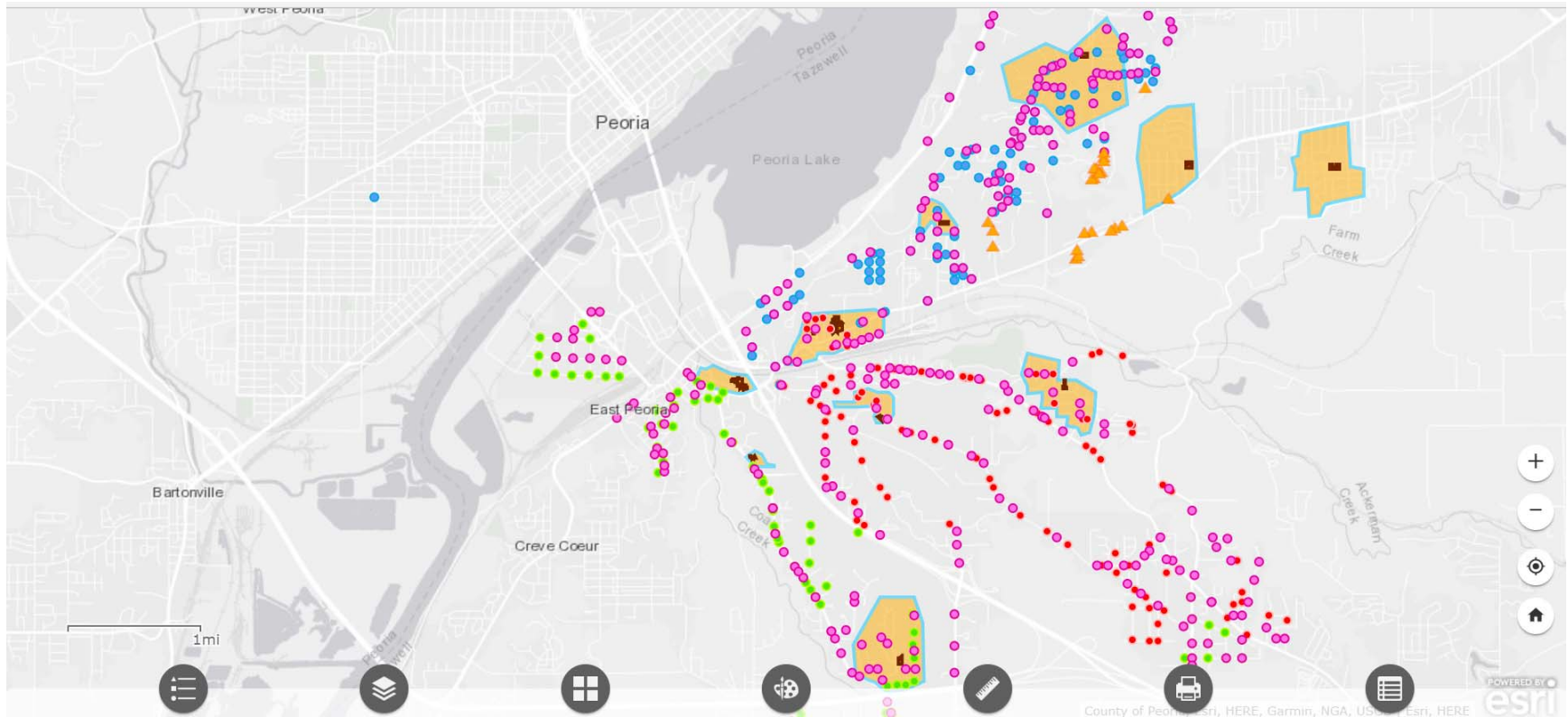
Veterans Drive - 1,926.2' / 0.36 Miles

## **Exhibit E**

## Map of School Walking Areas

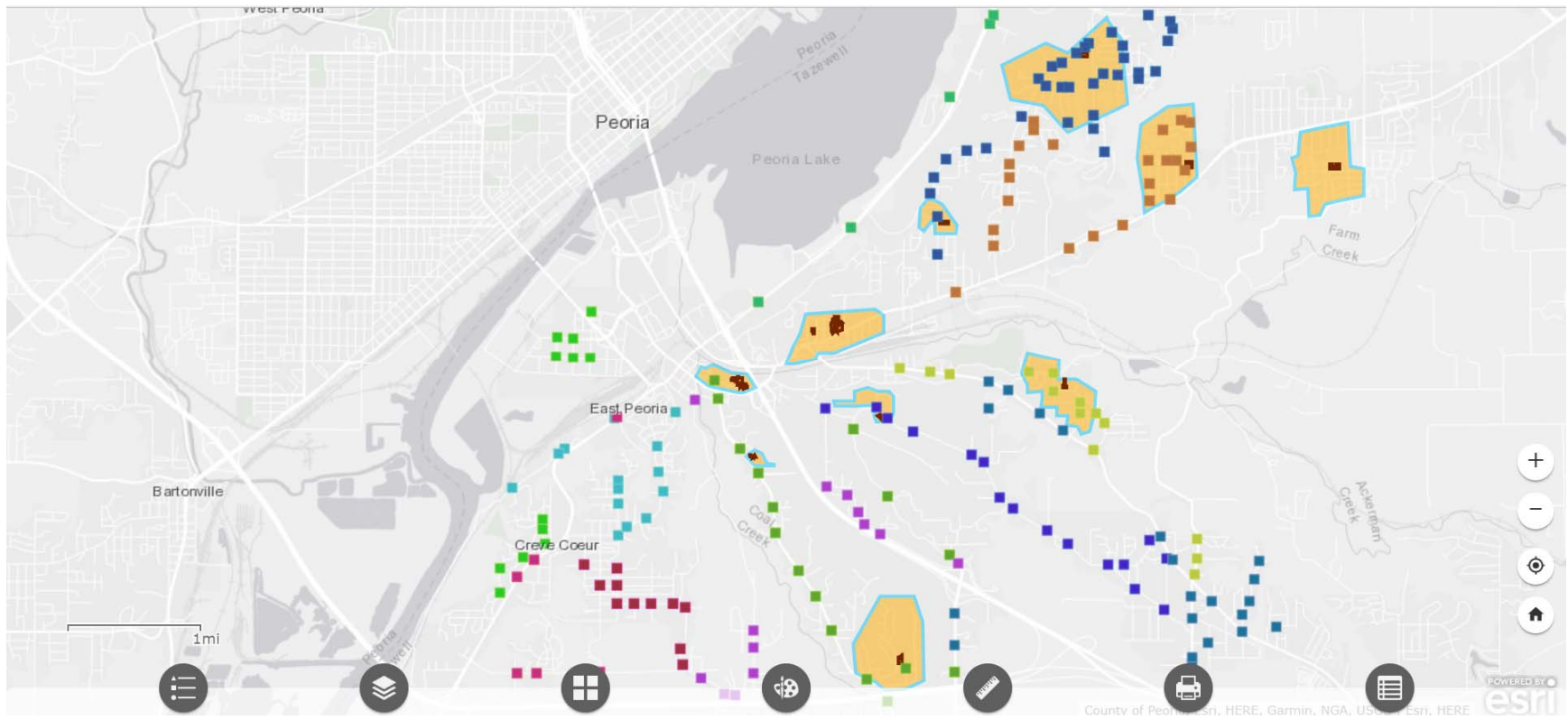


## Map of School Bus Stops for Elementary Schools



- ▼ ☒ District 85 - Robein School Bus Stops
- ▼ ☒ District 86 - Central Junior High School Bus Stop Locations
- ▼ ☒ Distict 86 - Elementary Bus Stop Locations
  - Armstrong-Bolin
  - Shute-Lincoln
  - Wilson-Glendale

## Map of School Bus Stops for High School



▼ ☒ District 309 - Bus Stop Locations (High School)

- 1
- 3
- 6
- 7
- 8
- 9
- 10
- 11
- 13
- 14
- 15
- 16

## **Exhibit F**

Website page for Link to Review this Report



## **Exhibit G**

Report an Issue with a Sidewalk

[Submit a service request](#)

**Here's how Access East Peoria works:**

1. See something that needs to be fixed like tall weeds, a pothole, broken street signs or lamps, graffiti, abandoned vehicles, illegal trash dumping, etc.?
2. Via the link above, submit a service request and even an image, sound clip or other media file relevant to the reported issue.
3. The relevant city staff members will receive the request and resolve the problem.

**Access East Peoria is not to be used for emergency situations as it is only monitored during business hours.**

**For emergencies, please dial 911.**

CALENDAR JUN  
22  
FRI

**CALENDAR OF EVENTS**  
**East Peoria Farmer's Market**  
3:00 PM - 6:00 PM

JUN  
25  
MON

**CALENDAR OF EVENTS**  
**Planning Commission Meeting**  
6:00 PM

JUN  
29  
FRI

**CALENDAR OF EVENTS**  
**East Peoria Farmer's Market**  
3:00 PM - 6:00 PM

[View All Events »»](#)QUICKLINKS 

**I want to...**

Select...



## **Exhibit H**

IDOT  
BUREAU OF LOCAL ROADS MANUAL  
SECTION 41-6

## **41-6 REQUIREMENTS FOR ACCESSIBLE PUBLIC RIGHTS-OF-WAY**

### **41-6.01 General**

Many highway elements can affect the accessibility and mobility of individuals with disabilities. These include sidewalks, curb ramps, parking spaces, buildings at transportation facilities, overpasses or underpasses, temporary traffic control, and landscaping or streetscaping. According to the *Americans with Disabilities Act* (ADA), 42 U.S.C. §12181 - 12189 and the *Illinois Environmental Barriers Act*, 410 ILCS 25/1 *et seq*, public highway right-of-way shall be made accessible to all users. Local Public Agencies (LPAs) are required to perform a self-assessment to determine barriers to accessibility within the public highway right-of-way.

Currently, a national uniform design guideline for public highway right-of-way has not been adopted by the United States Department of Justice and Federal Highway Administration (FHWA). However, the *Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG)* has been introduced by the United States Access Board through the federal rule-making process. Furthermore, FHWA has issued guidance that PROWAG may be used in order to comply with accessibility requirements. Therefore, the Department recommends that LPAs use the latest revision of PROWAG available at [www.access-board.gov](http://www.access-board.gov), when constructing or altering facilities within the highway public right-of-way.

Figure 41-6A provides a reference to criteria contained in PROWAG unless otherwise noted. The *ADA Standards for Accessible Designs* (ADAAG), the *Illinois Accessibility Code* (IAC), the *Uniform Federal Accessibility Standards* (UFAS), the *ILMUTCD*, and the *American National Standards Institute* (ANSI) should also be consulted as needed. Where LPA policies or local codes require criteria that exceed PROWAG, then the local criteria should be used.

### **41-6.02 Equivalent Facilitation**

The use of alternative designs, products, or technologies, that result in substantially equivalent or greater accessibility and usability than the requirements in PROWAG, is permitted. However, the LPA should document how the equivalent facilitation was determined.

### **41-6.03 Application**

All newly constructed facilities, altered portions of existing facilities, and elements added to existing facilities for pedestrian circulation and use located in the public right-of-way shall comply with the requirements in this Section.

**BUREAU OF LOCAL ROADS & STREETS**  
SPECIAL DESIGN ELEMENTS

41-6(2)

Oct 2013

Accessible Component	Scoping Requirements	Technical Requirements
Pedestrian Access Route (PAR)	R204	R302
Alternate PAR	R205	
Pedestrian Street Crossing	R206	R306
Curb Ramps and Blended Transitions	R207	R304
Detectable Warnings	R208	R305
Accessible Pedestrian Signals and Pedestrian Pushbuttons	R209	
Protruding Objects	R210	R402
Signs	R211	R410
Transit Stops and Transit Shelters	R213	R308
On-Street Parking	R214	R309
Off-Street Parking	ADAAG R208	ADAAG R502; IAC 400.310(c)
Passenger Loading Zones	R215	R310
Stairways and Escalators	R216	R408
Handrails	R217	R409
Doors, Doorways, and Gates	R218	
Ramps		R407

**SCOPING AND TECHNICAL REQUIREMENTS**

**Figure 41-6A**

**41-6.04 Alterations**

**41-6.04(a) General**

Where existing elements, spaces, or facilities are altered, each altered element, space, or facility within the scope of the project shall comply with the applicable requirements for new construction unless existing physical constraints exist. The alteration of multiple elements or spaces within a facility may provide a cost-effective opportunity to make the entire facility or a significant portion of the facility accessible.

Figure 41-6B provides a summary of some common alterations.

**BUREAU OF LOCAL ROADS & STREETS**  
**SPECIAL DESIGN ELEMENTS**

Oct 2013

41-6(3)

Type of Work	Alteration Required
Resurfacing (includes, but not limited to, addition of a new layer of asphalt, reconstruction, concrete pavement rehabilitation and reconstruction, open-graded surface course, microsurfacing and thin lift overlays, cape seals, and in-place asphalt recycling)	Correct non-compliant curb ramps and crosswalks
Remove and Replace Sidewalk (sidewalk patch, relocate poles or posts, etc.)	Replace with compliant (includes transition to non-compliant)
Curb & Gutter Replacement	Install curb ramps if replaced curb and gutter is at a pedestrian crossing with adjacent sidewalk
Pedestrian Signal (signal controller and software altered, or signal head replaced)	Installed Accessible Pedestrian Signals and accessible pushbutton
Restriping Parking Markings	Provide accessible spaces

**ALTERATION REQUIRED BY TYPE OF WORK**

**Figure 41-6B**

**41-6.04(b) Existing Physical Constraints.**

Where existing physical constraints make it impracticable for altered elements, spaces, or facilities to fully comply with the requirements for new construction, compliance is required to the extent practicable within the scope of the project. Existing physical constraints include, but are not limited to, underlying terrain, right-of-way availability, underground structures, adjacent developed facilities, drainage, or the presence of a notable natural or historic feature.

The LPA should document the various options considered, reasons for selecting, and any public comments received concerning an alteration to the extent practicable. Furthermore, the facility shall continue to be listed as a barrier in the LPA's self-evaluation.

**41-6.04(c) Transitional Segments.**

Transitional segments of an altered PARs shall connect to existing unaltered segments of pedestrian circulation paths. The transitional segment shall be removed when the existing unaltered section is altered in the future.

**41-6.04(d) Reduction in Access Prohibited.**

An alteration shall not decrease or have the effect of decreasing the accessibility of a facility or an accessible connection to an adjacent building or site below the requirements for new construction in effect at the time of the alteration. Sidewalk improvements that correct existing excessive cross slope should be carefully planned to avoid creating excessive slope in curb ramps or adding a step at existing building entrances. Solutions may include:

1. Split sidewalks that serve building entrances and street or highway at separate levels;

2. Sidewalks with greater cross slope along the curb and PARs with lesser cross slope along building fronts;
3. PARs along the curb and ramped entrances to buildings.

#### **41-6.04(e) Alterations to Qualified Historic Facilities.**

Where the State Historic Preservation Officer or Advisory Council on Historic Preservation determines that compliance with a requirement would threaten or destroy historically significant features of a qualified historic facility, compliance shall be required to the extent that it does not threaten or destroy historically significant features of the facility.

Where there is a federal agency “undertaking”, as defined in 36 CFR 800.16 (y), the requirements in section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and 36 CFR part 800 apply. Location of a facility within an historic district by itself does not excuse compliance with the requirements in this document. The State Historic Preservation Officer or Advisory Council on Historic Preservation must determine that compliance would threaten or destroy historically significant features of the facility. Reproductions or replications of historic facilities are not qualified historic facilities.

#### **41-6.05 Pedestrian Access Route**

A PAR shall be provided within sidewalks and other pedestrian circulation paths located in the public right-of-way. The pedestrian access route shall connect to accessible elements, spaces, facilities, and to other accessible routes not within the public right-of-way.

PARs shall consist of one or more of the following: sidewalks and other pedestrian circulation paths, or a portion of sidewalks and other pedestrian circulation paths; pedestrian street crossings and at-grade rail crossings; pedestrian overpasses and underpasses and similar structures; curb ramps and blended transitions; ramps; elevators and limited use/limited application elevators; platform lifts; and doors, doorways, and gates.

#### **41-6.06 Alternate Pedestrian Access Route**

When a pedestrian circulation path is temporarily closed by construction, alterations, maintenance operations, or other conditions, an alternate PAR shall be marked and/or provided. Any pedestrian barricades and channelizing devices shall comply with the *ILMUTCD*.

The *ILMUTCD* recommends that whenever possible, work should be done in a manner that does not create a need to detour pedestrians from existing pedestrian routes. Extra distance and additional pedestrian street crossings add complexity to a trip and increase exposure of risk to accidents.

#### **41-6.07    Curb Ramps and Blended Transitions**

A curb ramp, blended transition, or a combination of curb ramps and blended transitions shall be installed to allow pedestrians to continue through pedestrian street crossings, alleys, or driveways. The curb ramp (excluding any flared sides) or blended transition shall be contained wholly within the width of the crossing served. In alterations where existing physical constraints prevent a curb ramp at each crossing, a single diagonal curb ramp shall be permitted to serve both pedestrian street crossings.

There are two types of curb ramps:

- Perpendicular curb ramps have a running slope that cuts through or is built up to the curb at right angles, or meets the gutter break at right angles where the curb is curved. On large corner radiuses, it will be necessary to indent the gutter break on one side of the curb ramp in order for the curb ramp to meet the gutter break at right angles.
- Parallel curb ramps have a running slope that is in-line with the direction of sidewalk travel and lower the sidewalk to a level turning space where a turn is made to enter the pedestrian street crossing.
- Parallel and perpendicular curb ramps may be combined. A parallel curb ramp is used to lower the sidewalk to a mid-landing and a short perpendicular curb ramp connects the landing to the street.

Blended transitions are similar to curb ramps. However, blended transitions have a less severe running slope and some other differences from curb ramp design. Raised pedestrian street crossings are considered blended transitions. Blended transitions are suitable for a range of sidewalk conditions and should be considered at level intersections.

The Department has developed a series of highway standards that provide the requirements for curb ramp and/or blended transition construction. However, these standards may not provide the contractor and LPA construction supervisor with enough information to ensure accessibility compliance at locations with severe slopes, existing site constraints, or other unique situations. Therefore, the LPA should consider providing plan details for each corner where curb ramps or blended transitions are being constructed.

#### **41-6.08    Detectable Warning Surfaces**

##### **41-6.08(a)    General**

Detectable warning surfaces indicate the boundary between pedestrian and vehicular routes where there is a flush rather than a curbed connection and shall be provided at the following locations on the PAR and at transit stops:

- Curb ramps and blended transitions at pedestrian street crossings;
- Pedestrian refuge islands (minimum width 6 ft);
- Pedestrian at-grade rail crossings not located within a street or highway;
- Boarding platforms at transit stops for buses and rail vehicles where the edges of the boarding platform are not protected by screens or guards; and

- Boarding and alighting areas at sidewalk or street level transit stops for rail vehicles where the side of the boarding and alighting areas facing the rail vehicles is not protected by screens or guards.

Detectable warning surfaces should not be provided at crossings of residential driveways, since the pedestrian right-of-way continues across residential driveway aprons. However, where commercial driveways are provided with yield or stop control, detectable warning surfaces should be provided at the junction between the pedestrian route and the vehicular route.

#### **41-6.09 Pedestrian Street Crossing**

All pedestrian street crossings shall be accessible to pedestrians with disabilities. Where pedestrian signals are provided at pedestrian street crossings, they should consider accessible pedestrian signals

#### **41-6.10 Protruding Objects**

Objects along or overhanging any portion of a pedestrian circulation path shall not reduce the clear width required for the PAR.

#### **41-6.11 Signs**

Where audible sign systems and other technologies are used to provide information equivalent to the information contained on pedestrian signs and transit signs, the signs are not required to comply with accessibility requirements.

Audible sign systems and other technologies that provide information equivalent to the information contained on signs are more usable by pedestrians who are blind or have low vision. Remote infrared audible signs that transmit information to portable devices that are carried by and audible only to the user are an example of audible sign systems and other technologies.

#### **41-6.12 Transit Stops and Transit Shelters**

Where provided (regardless of who provides), transit stops and transit shelters shall comply with accessibility requirements.

Transit stops in the public right-of-way typically serve fixed route bus systems, including bus rapid transit systems, and light rail transit systems. Signs that identify the routes served by the transit stop shall comply with the technical requirements for visual characters on signs unless audible sign systems or other technologies are used to provide the information. The FHWA has issued guidance on the obligations of state transportation departments, metropolitan planning organizations, and transit agencies to coordinate the planning and funding of accessibility improvements to transit systems and facilities.

#### **41-6.13    On-Street Parking**

##### **41-6.13(a)    General**

Where on-street parking is provided on the block perimeter and the parking is marked or metered (including pay stations), accessible parking spaces shall be provided. Where parking pay stations are provided and the parking is not marked, each 20.0 ft (6.1 m) of block perimeter where parking is permitted shall be counted as one parking space. Figure 41-6C specifies how many accessible parking spaces shall be provided on the block perimeter where on-street parking is marked or metered.

Accessible parking spaces shall be identified by signs complying with the *ILMUTCD*. Accessible parking spaces should be located where the street has the least crown and grade and close to key destinations.

Total Number of Marked or Metered Parking Spaces on the Block Perimeter	Minimum Required Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 and over	4 percent of total

#### **ON-STREET PARKING SPACES**

**Figure 41-6C**

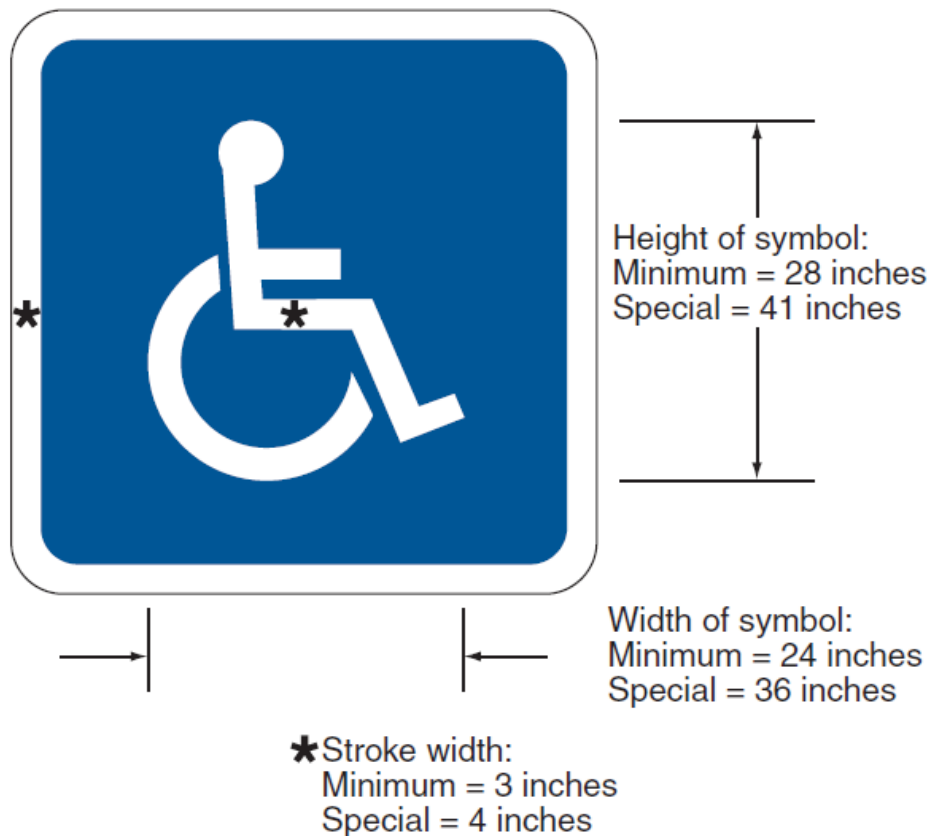
##### **41-6.13(b)    Signage**

Accessible parking spaces shall be designated with a ground-mounted RESERVED PARKING for persons with disabilities (R7-8) sign with the \$\_\_\_\_\_ FINE (R7-I101) supplemental plaque. The amount of the fine shall be as established by 625 ILCS 5/11-1301.3(c). Signs shall be 7 ft (2.0 m) minimum above the finish floor or ground surface measured to the bottom of the sign, and no more than 2 ft (0.6 m) horizontally from the face of curb. Signs shall be located at the head or foot of the parking space so as not to interfere with the operation of a side lift or a passenger side transfer.

##### **41-6.13(c)    Pavement Marking**

1.    Color. Accessible parking spaces and access aisles shall be designated with white pavement marking and may be supplemented with blue pavement markings.
2.    Width. Pavement marking shall be 3 in (75 mm) to 6 in (150 mm) wide.

3. Access Aisle. When required, the access aisle shall be marked with 45 degree diagonal lines. The diagonal lines should be 3 in (75 mm) to 6 in (150 mm) wide and separated by gaps of 12 in (300 mm) to 36 in (900 mm). The gap between the lines should not exceed 6 times the width of the diagonal lines.
4. Symbol. The international symbol of accessibility should be used to provide additional emphasis. See Figure 41-6D.



Note: Blue background and white border are optional

#### INTERNATIONAL SYMBOL OF ACCESSIBILITY FOR PAVEMENT MARKING

Figure 41-6D

#### 41-6.13(d) Dimensions

Where parking spaces are marked with lines, width measurements of parking spaces and access aisles shall be made from the centerline of the markings. The required size of parking spaces and access aisles shall be determined by the type of parking space.

#### **41-6.13(e) Curb Ramps or Blended Transitions**

Curb ramps or blended transitions complying with Section 41-6.07 shall connect the access aisle to the PAR. Curb ramps shall not be located within the access aisle. At parallel parking spaces, curb ramps and blended transitions should be located so that a van side-lift or ramp can be deployed to the sidewalk and the vehicle occupant can transfer to a wheelchair or scooter. Parking spaces at the end of the block face may be served by curb ramps or blended transitions at the pedestrian street crossing, provided that the curb ramp is not accessed from a travel lane.

Detectable warning surfaces are not required on curb ramps and blended transitions that connect the access aisle to the sidewalk, including where the sidewalk is at the same level as the parking spaces, unless the curb ramps and blended transitions also serve pedestrian street crossings.

#### **41-6.14 Off-Street Parking**

##### **41-6.14(a) General**

Accessible parking spaces for individuals with disabilities and accessible passenger loading zones that serve a particular building shall be the spaces or zones closest to the nearest accessible entrance on a PAR. In separate parking facilities that do not serve a particular building, locate parking spaces for individuals with disabilities on the shortest possible circulation route to an accessible pedestrian entrance of the parking facility. In buildings with multiple access entrances with adjacent parking, accessible parking spaces may be dispersed and located closest to the accessible entrances. The total number of accessible parking spaces may be distributed among parking facilities, if greater accessibility is achieved in consideration of such factors as anticipated usage, number and location of entrances and level parking areas. Figure 41-6E specifies how many accessible parking spaces shall be provided.

Each parking space shall have its own access aisle and all access aisles shall blend to a common level with an accessible route. Parking spaces and access aisles shall be level with surface slopes not exceeding 1:50 (2.0%) in all directions. Minimum vertical clearance of 98 in (2490 mm) at the parking space and along at least one vehicle access route to such spaces from site entrance(s) and exit(s) shall be provided.

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**SPECIAL DESIGN ELEMENTS**

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Total Number of Marked or Metered Parking Spaces on the Block Perimeter	Minimum Required Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2 percent of total
1001 and over	20, plus 1 for each 100, or fraction thereof, over 1000

**OFF-STREET PARKING SPACES**

**Figure 41-6E**

**41-6.14(b) Signage**

See Section 41-6.13(b).

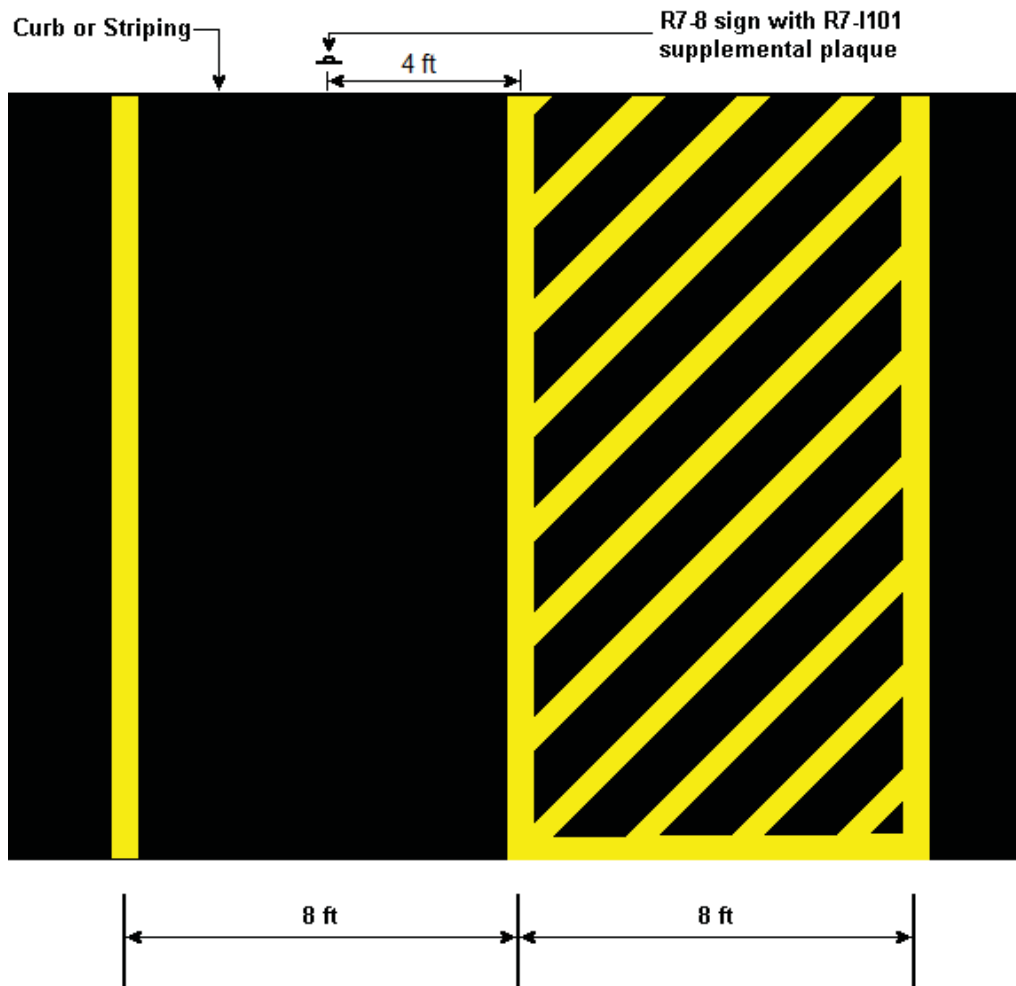
**41-6.14(c) Pavement Marking**

1. Color. Accessible parking spaces and access aisles shall be designated with yellow pavement marking.
2. Width. See Section 41-6.13(c).
3. Access Aisle. See Section 41-6.13(c).
4. Symbol. See Section 41-6.13(c).

**41-6.14(d) Dimensions**

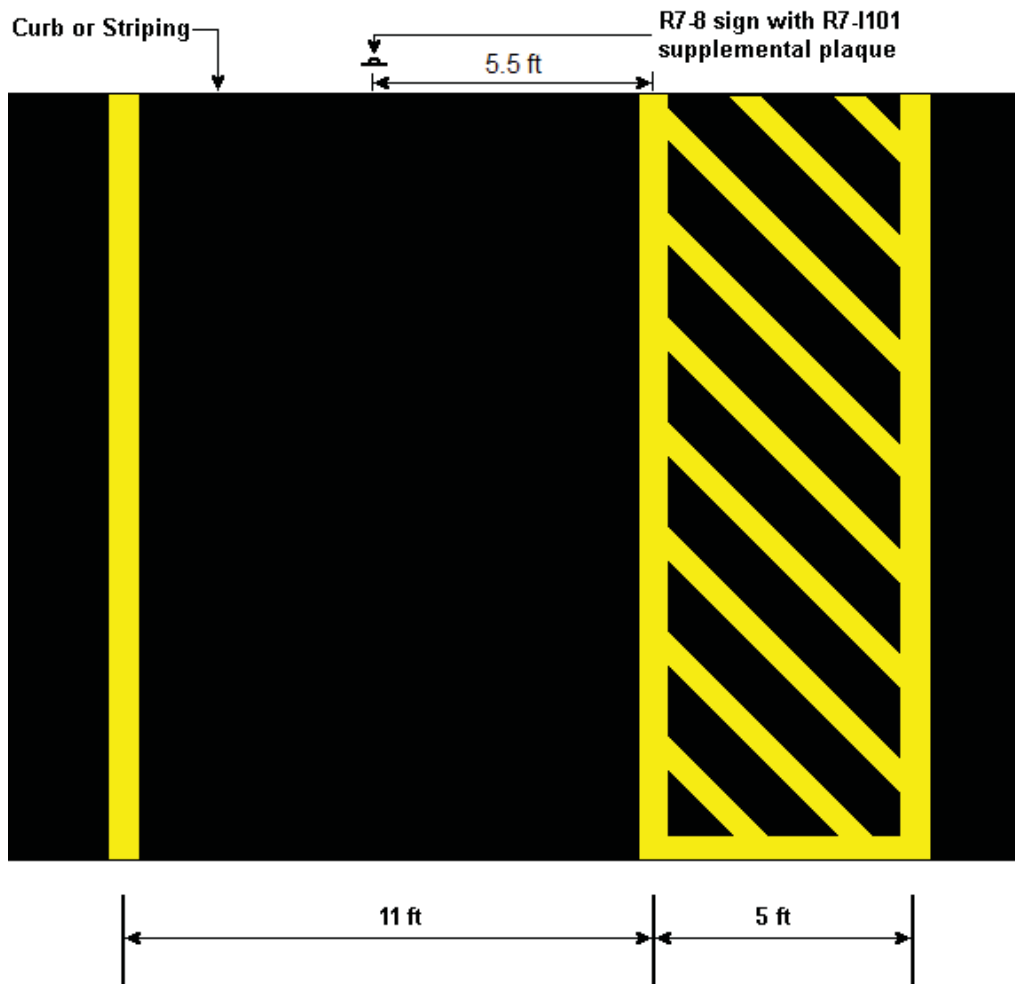
An accessible parking space shall consist of a 16 ft (4.9 m) wide parking space, which includes an 8 ft (2.4 m) wide access aisle (See Figure 41-6F). In the alternative, an accessible space may be 16 ft (4.9 m) wide, including a space 11 ft (3.4 m) wide with a 5 ft (1.5 m) access aisle (See Figure 41-6G).

A common access aisle shall not be shared between adjacent parking spaces. Access aisles shall be permitted to be placed on either side of the parking space except for angled parking spaces, which shall have access aisles located on the passenger side of the parking space.



**STANDARD OFF-STREET ACCESSIBLE PARKING SPACES**

**Figure 41-6F**

**UNIVERSAL PARKING DESIGN OFF-STREET ACCESSIBLE PARKING SPACES****Figure 41-6G****41-6.15 Passenger Loading Zones**

Where passenger loading zones other than transit stops are provided, at least one accessible passenger loading zone shall be provided for each 100.0 ft (30 m) of continuous loading zone space or fraction thereof. Accessible passenger loading zones must be identified by signs displaying the International Symbol of Accessibility.

**41-6.16 Other**

Where provided on pedestrian circulation paths, Stairways and Escalators (shall not be part of a PAR), Handrails, Doors, Doorways, and Gates; and Ramps shall comply with accessibility requirements.